

Persimmon Homes

# EASTON

## DESIGN CODE

November 2019



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## Section 1

# INTRODUCTION

### Background and Purpose

**This Design Code document has been prepared by FPCR Environment and Design Ltd on behalf of Persimmon Homes in advance of the Reserved Matters planning submission for ‘Land North and South of Dereham Road, Easton, Norfolk’.**

The proposal for: -

*“The erection of 890 dwellings; the creation of a village heart to feature an extended primary school, a new village hall, a retail store and areas of public open space; the relocation and increased capacity of the allotments; and associated infrastructure including public open space and highway works”*

(planning reference 2014/2611) was approved in outline on 1 November 2016.

The preparation of a Design Code in advance of the submission of the detailed proposals (reserved matters) is required by planning condition 33 of the outline approval which is worded as follows: -

*“Prior to the submission of any reserved matters application, a design code shall be submitted to and agreed in writing by the Local Planning Authority. This shall then be used to inform any subsequent reserved matters application.*

#### *Reason for condition*

*In order to ensure a satisfactory form of development that has appropriate regard for the locality. These details are required at this time as they are essential to guiding future reserved matters submissions”.*



Aerial Photograph

## Policy and Design Guidance

The preparation of the Design Code has been informed by and prepared in accordance with the following national and local planning policy and published design guidance.

### [National Planning Policy Framework \(NPPF\)](#)

The NPPF, updated 19 June 2019, defines a Design Code as: -

*“A set of illustrated design requirements that provide specific, detailed parameters for the physical development of a site or area. The graphic and written components of the code should build upon a design vision, such as a masterplan or other design and development framework for a site or area”.*

### [Planning Practice Guidance \(PPG\)](#)

Further guidance on using design codes is provided within the supporting PPG. This advises that: -

- *Preparing a good code is about finding a balance between technical specificity and a succinct description of what is required. Some of the best and most effective codes are very short.*
- *To promote speed of implementation, avoid stifling responsible innovation and provide flexibility, design codes should wherever possible avoid overly prescriptive detail and encourage sense of place and variety*
- *Codes should be succinct and carefully distinguish mandatory from discretionary components, avoiding ambiguous aspirational statements, unnecessary jargon and they should define any use of key technical terms.*

(Paragraph: 036 Reference ID: 26-036-20140306. Revision date: 06 03 2014)

### [The Joint Core Strategy for Broadland, Norwich and South Norfolk \(JCS\) 2011/2014](#)

**Policy 2 (Promoting Good Design)** requires all development to be designed to the highest possible standards, creating a strong sense of place and respecting local distinctiveness. It requires development to be evaluated against the Building for Life criteria published by CABE, achieving at least 14 points (silver standard)

### [South Norfolk Local Plan Development Management Policies Document \(Adopted October 2015\)](#)

This document includes a number of general policies that set out principles for good design, in particular DM3.8 (Design Principles) and DM4.9 (Incorporating landscape into design).

South Norfolk Local Plan Site Specific Allocations and Policies Document (Adopted October 2015)

Policy EAS 1 (Land South and East of Easton) sets out specific requirements for the Masterplan design that have been incorporated and approved at the outline application stage.

Easton Neighbourhood Plan 2017 – 2042, September 2017

The Plan outlines the Community Vision; *“To allow Easton to develop gradually and grow in a way that retains its natural beauty and character of a rural village to improve the quality of life for all generations”*. The vision is supported by a series of six objectives that relate to character, housing needs, protection of the countryside, improvement of local facilities, traffic and public transport infrastructure and involvement of local people in the development process.

Design Guidance

A number of supplementary planning (SPD) and design guidance documents have informed the preparation of this Design Code document, these include: -

- **South Norfolk Placemaking Guide SPD, September 2012**
- **Open Space SPD, September 2018**
- **Building for Life 12**
- **Manual for Streets**
- **The SuDS Manual, Ciria, 2015**
- **Safe, Sustainable Development**
- **National Design Guide, September 2019**
- **'At a Glance: a Checklist for Developing Dementia Friendly Communities', Housing Learning & Improvement Network, June 2012**
- **Transport for New Homes Charter**





## Format of the Document (Structure of the Code)

**Section 1** provides the background to the development proposals for the site and describes its existing surrounding context. The Design Code is underpinned by a series of outline approved drawings; Land Use, Density, Building Height and Illustrative Masterplan. These are updated by the Regulating Framework Plan.

**Section 2** of the Design Code introduces the Regulating Framework Plan, which has been developed based upon the Strategic Vision and Key Objectives for the development.

**Section 3** details the codes for each of the components shown on the Regulating Framework Plan; Land Use, Block Structure, Density, Building Heights, Movement, Street Hierarchy and Character Areas. These components determine the overall urban form for the development along with the Green Infrastructure described within Section 4.

**Section 4** is a key section describing the Green Infrastructure Principles for this landscape-led development. It details the codes for each of the key green infrastructure elements as follows; GI Principles, Soft Landscape, Play Spaces, Allotments, Sustainable Drainage Systems (SuDS) and Ecological Mitigation.

A series of linked key spaces have been identified throughout the development and are detailed within **section 5**. These are; Gateways, Green Spine, Easton Green, Neighbourhood Greens, Greenways, Pocket Parks, Focal Squares, A47 Corridor and Settlement Interface. The codes set out the distinctive urban form and design parameters of each of the identified spaces, with cross reference to the other relevant sections of the Design Code.

**Section 6** describes the codes that detail the place; Design & Materials – Built Form, Boundary Treatments, Parking, Public Realm & Street Furniture, Waste & Recycling and Other Key Design Considerations. The latter addresses Inclusive Access and Secured by Design as well as Building for Life.

Finally, **Section 7** addresses phasing and other management matters.

## The Site and its Context

The Site comprises 44.01 hectares of land adjacent to the southern, western and eastern fringes of Easton. The A47 borders the northern edge of the settlement, providing good connections with the city of Norwich, the centre of which is located approximately 9km east of Easton.

The settlement of Easton comprises predominantly post-war residential development, with pockets of more recent late 20th and early 21st century development. The Design and Access Statement provides a detailed analysis of neighbourhood character. These have informed the placemaking principles described in the following pages.

Within Easton retail provision is limited to a post office (open two mornings a week) and a fish and chip shop/restaurant. There is a primary school on the southern edge of the settlement adjacent to the site. Just outside of the settlement Easton gymnastics club is located on Bawburgh Road and adjoins the southern part of the site. The Church

of England Diocese of Norwich lies at the western edge of the village adjacent to the site and just south-east of the Grade I listed St Peter's Church. The Easton and Otley College entrance borders the eastern edge of the site. The Norfolk Showground lies further to the east. A development enterprise zone; The Food Enterprise Park, lies to the west of the settlement. The Longwater Retail Park and Park & Ride Interchange lie approximately 1.25km east of the site and north of the A47. Offsite highway works proposed for the development will further improve connections. The nearest high school is located to the east along Dereham Road. The Green Pedalway extends from Norwich city centre west to Bowthorpe employment area and is to be extended to Easton which will be a positive for the development.

The showground and college provide a range of recreational events and facilities on the doorstep for residents of Easton. A golf course lies just to the south east. In addition, the Easton College Woodland Walk provides walks through

the Four Acre Plantation (within the site), Fir Covert and the Brooms. Within the village, children's play areas include Jubilee Playing Field and smaller play areas within recent development on the southern edge of the village.

The Design and Access Statement analyses existing movement and circulation, landscape context, biodiversity and ecology, flood risk and drainage, utilities, noise, contamination and ground investigation, visual context, socio-economic context. These are summarised with a list of identified constraints and opportunities supported with summary diagrams (see the following two pages).

The context for development is well understood and has been covered in depth by the Design and Access Statement. Therefore, the Design Code references this where necessary, but does not include any additional material covering the contextual appraisal.



St Peter's Church

## Site Constraints

The following Constraints for the site are identified within the Design and Access Statement:

- *"Respecting existing neighbours boundaries*
- *Avoiding where possible development beyond the natural edge of the village to the west*
- *Retaining existing landscape features, plantations, and trees where possible*
- *Retaining existing hedgerows and field boundaries where possible*
- *Avoiding impact on ecology assets and mitigating where appropriate*
- *Exposed views to the south and east*
- *Undulating topography*
- *Noise constraint from the A47 and private road to Easton & Otley College\**
- *Address drainage constraints and avoid impact on surrounding land and properties*
- *A number of existing below ground and above ground utilities constraints to be addressed*
- *Existing allotments*
- *Existing buildings and the setting of Grade I Listed St Peter's Church*
- *Poor entrance into the village from the west"*



Properties backing onto the site



Existing Dereham Road gateway

\* Following consultation with Easton Parish Council an additional constraint has been identified relating to noise from the Showground. At reserved matters stage a full acoustic assessment will be undertaken to determine noise attenuation requirements along the eastern boundary of the site.



Constraints Plan (Outline Application Design and Access Statement)

## Site Opportunities

The following Opportunities for the site are identified within the Design and Access Statement:

- *"The provision of private and affordable housing (social rent/discounted market value)*
- *Opportunity to provide a new village hall and shop*
- *Opportunity to provide a significant contribution to the affordable housing needs in the local area*
- *Opportunity to respond to the existing character areas to have a positive impact on the setting and sense of place*
- *Create a new gateway and arrival from the east*
- *Respect the setting of St Peter's Church*
- *Provide a new village centre*
- *Provide significant amount of open space, recreation and plays areas for new and existing residents*
- *Create new safe paths through the development connecting to the countryside*
- *Provide structural planting to mitigate exposed views*
- *Provide structural planting and acoustic measures to mitigate noise*
- *Provide safe new paths to Easton & Otley College for students, staff and general public*
- *Provide land for the expansion of the existing primary school*
- *Provide a sustainable drainage system to attenuate water and avoid impact on surrounding areas*
- *Provide connections to the land at Deer Park House for future Gym Club access*
- *Provide additional space for allotments"*



Opportunities Plan (Outline Application Design and Access Statement)

## Placemaking Principles

The South Norfolk Placemaking Guide SPD and the Easton Neighbourhood Plan 2017 – 2042 provide specific guidance and policy that have been taken into account in the production of the Design Code for the development. Key information is summarised below for ease of reference: -

### [South Norfolk Placemaking Guide SPD, September 2012](#)

The Placemaking Guide SPD describes the general vernacular architecture of South Norfolk, landscape character, the character of key areas for growth which include Easton and ecology & biodiversity. It then addresses place-making and design principles before advising on the design process. The purpose of the guidance is to influence and raise the quality of design and layout of new development in South Norfolk.

The key characteristics of relevance to Easton and the development are summarised adjacent: -

### *Key Vernacular Characteristics of South Norfolk*

**Materials** – Typical traditional materials include flint (churches), plaster (plain, colour washed or decorated with a raised or incised pattern), timber, clay lump, soft red bricks (English bond and later on Flemish bond), white/gault bricks, black or red/orange shallow ‘S’ profile pantiles, thatch and uncommonly slate roofs.

**Details** – Buildings are generally less decorated than those of North Suffolk and there is less use of flint than in North Norfolk. Details include brick detailing to gables, decorative brick chimney stacks, decorative brick layout at eaves level, brick patterning, horizontal weatherboarding, range of roof pitches some quite steep (pantile formerly thatch) and occasional lower pitched welsh slate roofs, verges detailed with a top cover (some decorative) bargeboard or finished against a protective parapet, ridges are traditionally half round ridge tiles, dormer windows (wedge or gabled) are a typical feature.

**House plan and form** – traditionally older properties were a single room depth rectangular plan, whereas by the 18th century double depth ‘square’ plan with shallower pitched roofs became typical.



*Older properties within Easton*

### *Landscape Character*

Easton falls within the Mid Norfolk National Landscape Character Area. The key characteristics for this area include;

*" Variable geology, with extensive sand and gravel soils; Predominantly arable, with variable field sizes, generally medium rather than large; relatively well-wooded, often a reflection of sporting interest within the estates, but with little ancient woodland."*

The South Norfolk Landscape Character Assessment locates the site within Character Area G1: Easton Fringe Farmland. The key characteristics for this area include;

- " Undulating landscape sloping towards a distinct ridge top.*
- Highly developed ridge top with a strong urban fringe character including the presence of significant urban settlement at Costessey and the smaller linear settlement of Easton, large retail superstores with their associated car parks and a park and ride scheme.*

- Large scale recreational uses including the presence of a golf course and the Royal Norfolk Showground.*
- Strong rural character of arable and pastoral farmland context but dominated by urban development.*
- Major transportation through-route and gateway into South Norfolk from the southern bypass.*
- No scheduled monuments or historic parklands within the landscape and absence of distinctive built elements or historic features such as round towered or isolated churches."*

The 'Key Design Principles for Area G1 include; *"Ensure that good quality rural views from the ridge top to the surrounding countryside are preserved; Consider the views from the surrounding landscape which is highly sensitive to any development on or near the prominent ridge top; Consider the impact of proposals upon the quality of the 'sense of arrival' into South Norfolk and how new development will enhance this."*



*Recent residential development in Easton*

**EASTON** DESIGN CODE

### *Easton Settlement Character*

*“Easton originated as a linear village along a street and was a post-medieval settlement clustered on a crossroads east of St Peters Church. Today it is largely a late c20 development to the south of the A47 with the church isolated from the settlement to the west side”* (page 89).

Key characteristics for Easton include: -

- *“ Plateau stretching south from the A47. River valleys to the north and northeast with dense woodland and undulating landscape.*
- *Some surviving ancient woodland south of Easton. Amalgamated field pattern east and northeast of Easton.*
- *Brick based architecture including Costessey white brickwork with distinctive patterns and chimneys. Other materials include, flint, painted render, clay pantiles.*
- *Enclosing brick walls, plinths, some traditional iron railings*

- *Mature trees, hedgerows are a feature in the modern settlement of Easton.*
- *A47 provides good access from south of Norwich, through Easton to Dereham. Lack of major roads north to south helps to retain natural character of countryside but there is heavy traffic on the narrow roads through historic parts of Easton,*
- *Amalgamation of some fields around Easton has removed historic settlement pattern. The landscape to the south of Easton has variable field sizes, a large number of small to medium sized 18th century estates; some surviving ancient woodland; dispersed villages and isolated farmsteads within a complex minor road system; cohesive 17th/18th century vernacular architecture.*
- *Modern residential development in parts of Easton, Fairfield Park and Queen’s Hills lack distinctive identity with the majority of houses and apartment buildings being of a standard design.”*

Key Design Principles for the area include: -

- *“ Ensure that new development responds to the scale, form and proportions of existing vernacular buildings in the historic areas reflecting the existing distinctive character.*
- *New development respects the pattern of existing settlements in distinctive character areas.*
- *Materials should respond to existing finishes although new materials could be introduced with more contemporary designs.*
- *Scale, form, material finishes respect the vernacular character of existing buildings.*
- *Positioning of individual buildings and layout of groups of buildings is sympathetic to the landscape pattern and character.*
- *Respond to boundary treatments in distinctive character areas.*
- *Incorporate trees to provide a natural back drop to groups of buildings and to break up the built form.”*



*Easton Oblique Aerial (taken from the South Norfolk Placemaking Guide)*

## Section 2

# VISION AND DEVELOPMENT FRAMEWORK

### Strategic Vision

The Design and Access Statement outlines the key design principles for the site and this includes the identification of the key spaces that help to define the character of the development (as indicated on the adjacent approved Illustrative Masterplan). These principles are carried forward and developed further within the Design Code, helping to define the Vision, Objectives and subsequent Regulating Plans and Design Codes.

**The overall vision for the development of the site is to provide a distinctive and sustainable, high-quality place, which relates positively to Easton and its surroundings, reflecting local distinctiveness and drawing its sense of place from the local heritage, landscape and biodiversity.**

### Key Design Principles

The following key design principles respond to the headings within **Section 3** of the South Norfolk Placemaking Guide and define the background approach to the Design Codes set out within this document:

- **Community Facilities:** A range of well-designed new and improved, accessible social and community facilities at the heart of Easton.
- **New Homes:** A balanced neighbourhood carefully laid out to be compatible with the existing settlement and that delivers sustainable homes with varying types and sizes of accommodation and tenure that contribute towards meeting local needs.
- **Accessibility & Transport:** A new place defined by sustainable transport options.
- **Integrate with Surroundings:** A well-integrated place with a clear legible hierarchy of safe, convenient and attractive streets and footpaths that are accessible to all, dementia friendly, and well-linked to the surrounding settlement and rural edges.
- **Existing Features:** A new place that respects, retains and where possible, enhances its heritage, landscape and biodiversity setting.
- **Green Infrastructure:** A new place defined by enhanced green infrastructure which will incorporate

existing features and provide a variety of multi-functional and connected open and green spaces, sustainable drainage and bio-diverse habitats.

- **Character:** A new place defined by locally distinctive buildings, streets and open spaces which vary in character through a coherent design response to context and location.
- **Development Form:** A well-defined place with buildings of appropriate scale and mass, continuous frontages, views and vistas to landmarks, gateways and focal points, with well-integrated, non-dominant parking solutions, a clear distinction between public and private space, and visually well-connected public spaces.
- **Secured by Design:** New buildings will be orientated to overlook the new public realm of streets, parking, pedestrian routes and open space, with entrances orientated towards the street to create active frontages. Based on local precedent street lighting is not proposed. The decision as to whether street lighting will be required will be taken by the Highway Authority and Easton Parish Council when they consider the Reserved Matters applications. Should street lighting be required care will need to be taken to ensure that dark edges are maintained with the rural interface and within the north-west of the site to avoid impact on bats.



## The Outline Consent

The outline approved Illustrative Masterplan is supported by a series of approved Parameters Plans. These are indicated adjacent for ease of reference.



*Approved Parameters Plans*



## DEVELOPMENT STRUCTURE

	Residential Development	<b>C3.1:</b> Land Use & Phasing <b>C3.2:</b> Block Structure <b>C3.3:</b> Density <b>C3.4:</b> Building Heights	
	School Extension Land		
	Shop		
	Village Hall		
	Development Block Frontage		
	Focal Buildings		
	Cycle/Pedestrian Route		<b>C3.5:</b> Movement <b>C3.6:</b> Green Spine <b>C3.7:</b> Secondary Street <b>C3.8:</b> Lanes <b>C3.9:</b> Shared Private Drives
	Existing Recreational Footpaths		
	New Recreational Footpaths		
	Footpath and Cycle Connections		
	Green Spine		
	Secondary Street		
	Lanes		
	Shared Private Drives		
	Village		
	Rural		
	Contemporary		

## GREEN INFRASTRUCTURE

	Public Open Space	<b>C4.1:</b> GI Principles <b>C4.2:</b> Ecological Mitigation <b>C4.3:</b> Soft Landscape
	Existing Vegetation	
	Proposed Structural Planting	
	Ecological Mitigation Area	<b>C4.4:</b> Play Spaces
	Play Areas	
	New Allotments	<b>C4.5:</b> Allotments
	SuDS Feature or Basin	<b>C4.6:</b> SuDS
	Proposed Swales	

## KEY SPACES

	Key Spaces	<b>C5.1A-5.1C:</b> Gateways <b>C5.2:</b> Green Spine <b>C5.3:</b> Easton Green <b>C5.4:</b> Neighbourhood Greens <b>C5.5:</b> Greenways <b>C5.6:</b> Pocket Parks <b>C5.7:</b> Focal Squares <b>C5.8:</b> A47 Corridor <b>C5.9:</b> Settlement Interface
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## Section 3

# DEVELOPMENT STRUCTURE

### Land Use

The development is predominantly residential development accessed via a central spine road and with green infrastructure. A central public green is bordered by land reserved for a village hall, school extension and shop. The proposed green infrastructure creates a buffer between the housing and the countryside and provides greenspace linked by a green network of recreational footpaths and pedestrian routes. The proposed green infrastructure includes children's play spaces (8 LAPs, 2 LEAPs and a NEAP), SuDS and community allotments (relocated and enlarged).

CODE 3.1	Land Use	Area
<b>Residential</b>	890No dwellings are proposed to be delivered across 5 phases. A broad mix of house types will include 1-5 bedroom houses. The housing mix will include affordable housing as defined within the S106.	c. 25.6Ha
<b>Green Infrastructure</b>	A range of provision including SuDS features, semi-natural greenspace, amenity greenspace, recreational and play facilities and allotments, all linked via a pedestrian footpath network (refer to Section 4 for details).	c.14.16Ha
<b>Spine Road</b>	The primary road through the development is set within a c. 19.5m wide corridor.	Included within Residential
<b>Land reserved for other uses</b>	0.85Ha of land is reserved for a primary school extension and a further 0.16Ha for a village hall and 0.17Ha for a shop.	c. 1.18Ha



- Residential Development (inc. highways) - 25.6Ha
- School Extension Land - 0.85Ha
- Village Hall Land - 0.16Ha
- Shop - 0.17Ha
- Allotments - 1.46Ha
- Swales - 0.95Ha
- POS (inc drainage basins) - 9.43Ha
- Structural Landscaping - 2.32Ha

*Land Uses*

## Block Structure

The structure of the development is designed to set out a clear hierarchy of streets and to complement a linked network of key spaces. A robust, clearly defined closed block structure will be implemented in accordance with the South Norfolk Place Making Guide, Secured by Design Principles and Building for Life criteria (as required by Policy 2 of the JCS). The block dimensions vary but are generally 40-50m wide and of varying block length.

The block structure has been designed to ensure a permeable development that allows ease of movement and connectivity throughout the development via a clear hierarchy of streets and green spaces accessed via the main spine road.

Blocks will vary subtly in terms of the enclosure they create (as the density varies) but will be designed to ensure they consistently provide a clear distinction between public and private spaces. Blocks will become looser in character and more organic in form towards the edges of the development, particularly where they overlook the greenway and gateway spaces.

A continuous or semi-continuous frontage of new buildings is proposed, orientated to overlook the new public realm of streets, parking, pedestrian routes and open space, with entrances orientated towards the street to create active frontages and to minimise risk of crime and anti-social behaviour through natural surveillance.

Architectural rhythm will be a consideration where appropriate within the design; patterned repetition and alteration within the built form, including the incorporation of focal buildings at key focal points and to terminate vistas, will help create legible streets and a sense of place.

CODE 3.2	Block Structure
<b>Key Design Principles</b>	Closed block arrangement to accord with South Norfolk Place Making Guide, Secured by Design Principles and Building for Life.
	Permeable development that allows for ease of movement, with connected streets and footpath/cycleway network.
	Block enclosure to be designed to ensure a consistent clear distinction between public and private spaces.
	Blocks will become looser in character and more organic in form towards the edges of the development.
	Provide a continuous or semi-continuous frontage of buildings to streets – majority of properties to overlook the street or greenspaces (occasional gables).
	Safe and secure plots have regard to the principles of ‘Secured by Design’ to reduce opportunities for crime.
Good design to incorporate architectural rhythm to frontages where appropriate and incorporate focal buildings at key focal points and to terminate vistas.	



- Development Blocks
- Development Block Frontage
- \*
 Focal Buildings

*Block Structure*

## Density

The residential layout will be designed to provide a range of densities across the site in accordance with the Design and Access Statement and approved parameter plans. The range of densities is shown on the adjacent plan.

Generally, higher densities will be located within the heart of the development and alongside the spine road, becoming lower in density to the periphery of the development, and very low density within the north-west within the vicinity of the St Peter's Church and Diocese buildings. An exception to this principle is within the north-east of the development, higher density dwellings are proposed along the northern development edge with the A47, adjacent to the proposed noise buffer, where they will function as a further noise barrier to gardens and streets to the south.

CODE 3.3	Density
Key Design Principles	High density areas within the heart of the development, alongside the spine road and alongside the A47 noise buffer (c. 40-50dph net).
	Remaining areas to be medium density (c. 30-40dph net).
	Low density areas to the peripheries of the development (c. 25-30dph net).
	Very low density areas within the north-west corner of the site within proximity of St Peter's Church and the Diocese buildings (c. less than 25dph net).



- Very Low Density - <25dph\*
- Low Density - 25-30 dph\*
- Medium Density - 30-40 dph\*
- Higher Density - 40-50 dph\*

\* All densities shown are net.  
Overall gross density for the development - 22dph

*Density*

## Building Heights

The height of the dwellings proposed will reflect the precedent within existing parts of Easton and the density framework defined in **Code 3.3**. New dwellings will be predominantly 2-storeys in height. Some limited 2.5 storey is proposed to provide variety on the street scene along the spine road and at focal points including adjoining focal squares. Along the site peripheries adjoining existing property boundaries, some new dwellings will be bungalows.

The adjacent plan shows the proposed distribution of building heights.

CODE 3.4	Building Heights
Key Design Principles	Predominantly 2-storeys.
	Limited 2.5 storeys along the spine road and at focal points within the street scene.
	Bungalows will generally be proposed where they adjoin existing property boundaries in accordance with the Building Heights Plan.



- Single Storey
- 2 Storey
- 2 Storey with occasional 2.5
- Predominantly 2.5 Storey

*Building Heights*

## Movement

A permeable development that prioritises ease of movement for pedestrians and cyclists is proposed. The adjacent plan shows the proposed key vehicular links and key pedestrian / cycle links how they connect with the wider network.

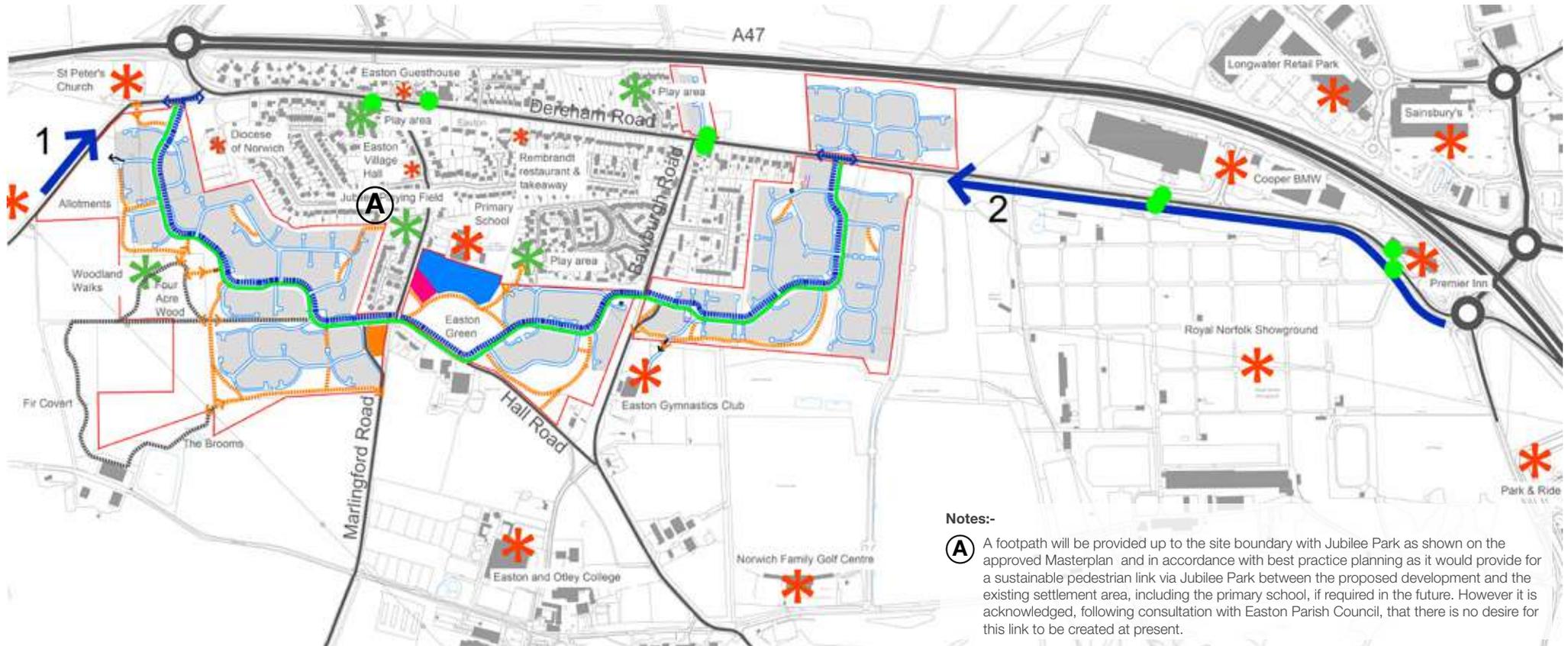
Existing vehicular routes west, south and east out of Easton are incorporated and adapted to successfully integrate the development with its surroundings. This includes careful and sensitive design of the vehicle access from the lane close to St Peter's Church, speed limiting changes to Dereham Road, with the 30mph speed limit extending from the current settlement edge eastwards across the Easton gateway / development access. The main route through the development (the Green Spine) will be designed to accommodate future bus routes / extension of existing routes. There will be some adjustments made to road alignments and connections in the south where they are crossed by the Green Spine route. Overall a low-speed environment of 20mph or less is proposed throughout the development.

A direct and attractive pedestrian / cycle network alongside the Green Spine through the development is proposed, from here direct connections to bus stops and to adjoining

greenspace networks both within and adjacent to the site are proposed, including to existing concessionary circular walks that pass through Four Acre Wood.

Within the wider context city-wide the development would provide a dedicated foot/cycle link from the development, along Dereham Road and over the Longwater Interchange. This would provide a much needed safe access for local people to key locations such as Longwater Retail Park (1.25km from the site), Costessey Medical Centre and Ormiston Victory Academy. Further to the committed infrastructure related to this development, Norfolk County Council have published proposals for a funding bid to extend the Green Pedalway (which currently runs from Norwich city centre to Bowthorpe employment area) to Easton via a new foot/cycle bridge over the A47 at Long Lane, which would link to the new footway/cycleway link between this proposed development and the Longwater Interchange. This would provide an alternative route for commuting to Bowthorpe, Norwich and beyond to Broadland Business Park. Additionally this would create a link to the existing park & ride facility which provides bus services to the local hospital (NNUH) and University of East Anglia. The proposed development would also provide an opportunity for public transport operators to create a circular route within the extended village footprint.

CODE 3.5	Movement
Key Design Principles	Movement network to be designed for pedestrian / cycle priority.
	To comprise permeable streets that connect sensitively into the existing highway network.
	Green Spine to be designed to accommodate buses.
	Reduction in speed limit to 30mph on Dereham Road from the current edge of Easton to the new gateway entrance.
	Design for maximum 20mph speeds throughout the development.
	A clear pedestrian / cycle network to be provided along green routes and linked green spaces, with connections to existing streets and green spaces.
	Pedestrian links into the existing concessionary circular footpaths to the south-west of the site via Four-Acre Wood.
Benefits from links to Longwater retail park and further city-wide sustainable transport connections and the proposed extension of the Green Pedalway cycle route (Easton to Norwich City centre) which would also provide a pedestrian/cycle link to Park and Ride facility.	



- |                              |                                 |   |              |
|------------------------------|---------------------------------|---|--------------|
| Existing Road Network        | Existing Recreational Footpaths | Strategic Pedestrian / Cycle Connections                            | Destinations |
| Primary Street (Green Spine) | New Footpath Connections        | <b>1</b> New Footpath to be implemented by the Food Enterprise Park | Green Spaces |
|                              | New Cycle/Pedestrian Route      | <b>2</b> Proposed Strategic Green Pedalway Extension                | Bus Stops    |

**Notes:-**

**(A)** A footpath will be provided up to the site boundary with Jubilee Park as shown on the approved Masterplan and in accordance with best practice planning as it would provide for a sustainable pedestrian link via Jubilee Park between the proposed development and the existing settlement area, including the primary school, if required in the future. However it is acknowledged, following consultation with Easton Parish Council, that there is no desire for this link to be created at present.

*Movement Connections and Links*

## Street Hierarchy

A clear hierarchy of streets provides a legible environment. The development will be based on four street character types. Access is via a primary Green Spine road route through the site. Secondary streets link to the Green Spine road at key junctions and lead to Lanes and Shared Private Drives. The street types also will give the development character and identity and provide a sense of place.

Street types will have different design characteristics, in terms of function, width, building form, landscape, and frontage details. The Green Spine for example, is the principal street within the layout and will be a wide street, with space for a swale, grass verge and street trees and will be more formal in design and represented by 2 storey and a proportion of 2.5 storey detached, semi-detached and linked / terraced dwellings with shallow and narrow private frontages.

In contrast, the Lanes and Shared Private Drives in lower density areas will be designed, where feasible with a shared surface street and a more informal building arrangement. Shared Private Drives will be represented by 2 storey detached and semi-detached dwellings with deeper and wider private frontages. This will help to define the Shared Private Drives as the most minor streets within the layout.

The exact alignment of the streets and pedestrian routes should seek to retain and respond to site features such as hedgerows, trees and topography, wherever possible. All streets will be designed in accordance with Manual for Streets and to adoptable standards, approved by Norfolk County Council Highway Authority.



*Typical Green Spine*



*Typical Secondary Street*



*Typical Lane*



*Typical Shared Private Drives*

*Typical images: Street Hierarchy and Connections*



- Green Spine
- Secondary Street
- Lanes
- Shared Private Drives
- Cycle/Pedestrian Route
- Existing Recreational Footpaths
- New Recreational Footpaths and Connections

*Street Hierarchy*

## Green Spine

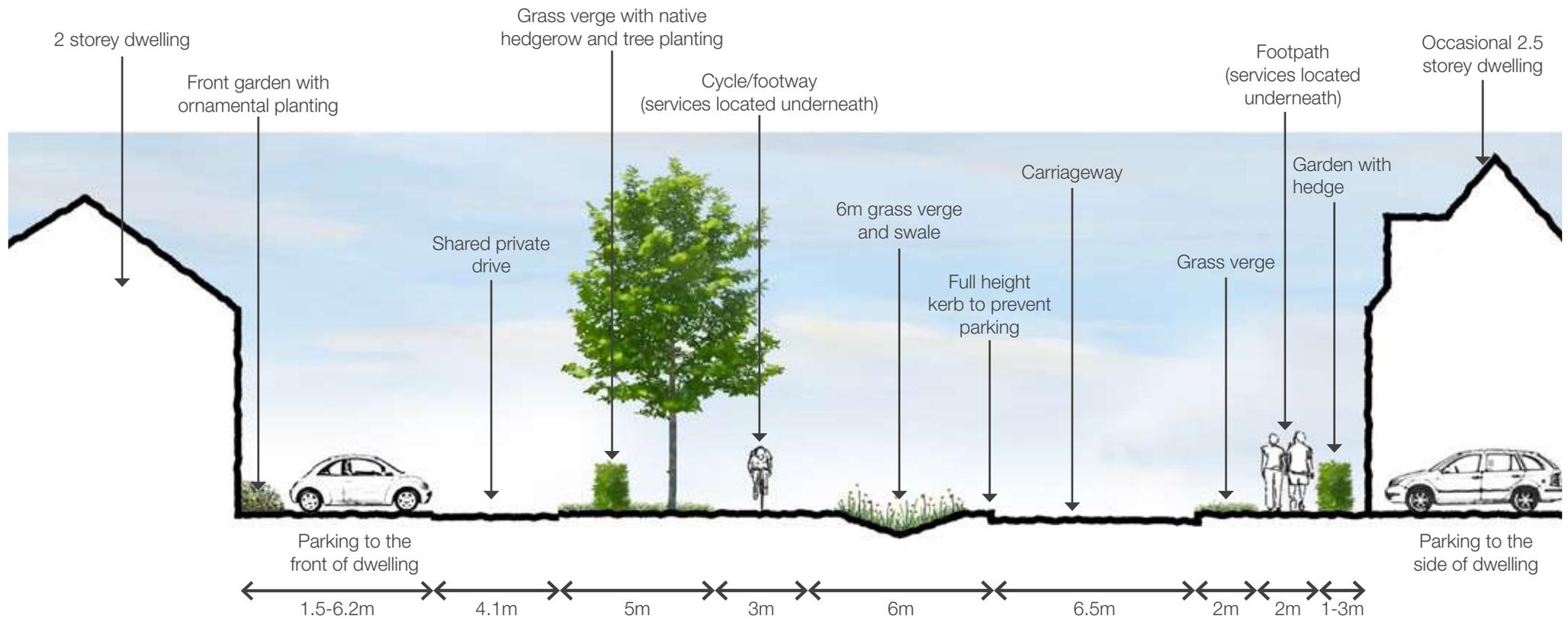
The Green Spine leads into the development from Dereham Road and loops centrally through the development connecting into the existing lane near St Peter's Church, close to the western end of Dereham Road. It provides connections to the side streets (secondary streets and lanes). The Green Spine is designed to provide safe movement for all pedestrians, cyclists and vehicles.

The Green Spine incorporates a footpath to one side and a combined foot/cycleway to the other, set back from the road behind landscape corridors which will include grass verges, a swale and trees on the street scene. The Green Spine passes through and connects areas of greenspace, parks and incorporates two focal squares at key junctions and points of arrival. The Green Spine is fronted on both sides by built development although development to one side of the street is set back beyond the landscape corridor and accessed via frequent shared private drive crossings over the swale.

### Key Design Principles:

- **Gateway street with landscape corridor**
- **Landscape corridor incorporates grass verges, swale and street trees**
- **Connects areas of greenspace**
- **Provides the main vehicle access – wide carriageway**
- **Designed to provide priority for pedestrian / cycles and buses**
- **Footpath to south side and combined foot/ cycleway to north side, separated from the street by a grass verge or swale**
- **Vehicle speeds restricted to 20mph**
- **Changes of highway surface at key junctions and pedestrian crossings**
- **Geometric design, vertical and horizontal features and traffic calming to keep vehicle speeds down**
- **Where possible pedestrians and cyclists will be given priority at junctions**
- **Hedgerows that cross the Green Spine will be utilised to create natural pinch-points – landscaped traffic calming approach**
- **Street lighting is not proposed, if required it will be sensitive to setting**
- **Occasional frontage parking to one side of the street only**
- **Opposite the swale a strong single building line of predominantly narrow plan dwellings with shallow plot frontages actively facing the street**
- **Adjacent to the swale a lower density arrangement of dwellings accessed via shared private drives, will actively face the street, reflective of the character of new development along Dereham Road frontage**
- **Primarily 2-2.5 storey dwellings, 2.5 storey will be utilised to vary the roofline and provide focal buildings**
- **Landmark buildings and trees at key junctions and terminating vistas**

## Green Spine Indicative Section



Green Spine



CODE 3.6	Green Spine	
Design Speed	20mph.	
Carriageway Width	6.2 - 6.5m.	
Footpath	2m (south side).	
Cycleway	3m (north side).	
Verge	Generally 2m grass verge. Generally 6m verge with swale.	
Trees	Trees on the street scene will be provided at an average rate of no fewer than 6 per 50 metres. Trees must not be located within a required visibility splay or within 5m of a streetlight (if provided). Refer to the Landscape Framework Plan and to Code 5.2 for further information on trees.	
Parking	Varies: On plot parking within private driveways and garages, positioned opposite the swale and served off shared private drives adjacent to the swale. Frontage parking for continuous frontage properties adjacent to the swale, 90-degree, maximum 4 spaces per group with soft landscaping between groups.	
Buildings	Street Character Principles	A semi continuous building line. Properties to front the street. A mix of terrace, semi-detached and detached properties. Further detail provided within Design Code 5.2.
	Dwelling Form	Narrow and some wide plan.
	Dwelling Height	Primarily 2-2.5 storey dwellings.
	Dwelling Materials	Shown within Design Code 6.1.
Boundary Treatment	Private frontages 1-3m (and up to 6.2m where frontage parking is proposed) depth opposite the swale defined by clipped hedgerows / planting areas within front gardens (as appropriate to the character area in which they are located) or walls where adjoining buildings or garages.	
	Between 1.5-6.2m (minimum) adjacent to the swale, with lawns and ornamental planting.	
Materials	Carriageway: Accord with NCC specification. Footpath: Accord with NCC specification. Combined Foot/Cycleway: Accord with NCC Specification.	
Services	Within highway corridor.	

## Secondary Streets

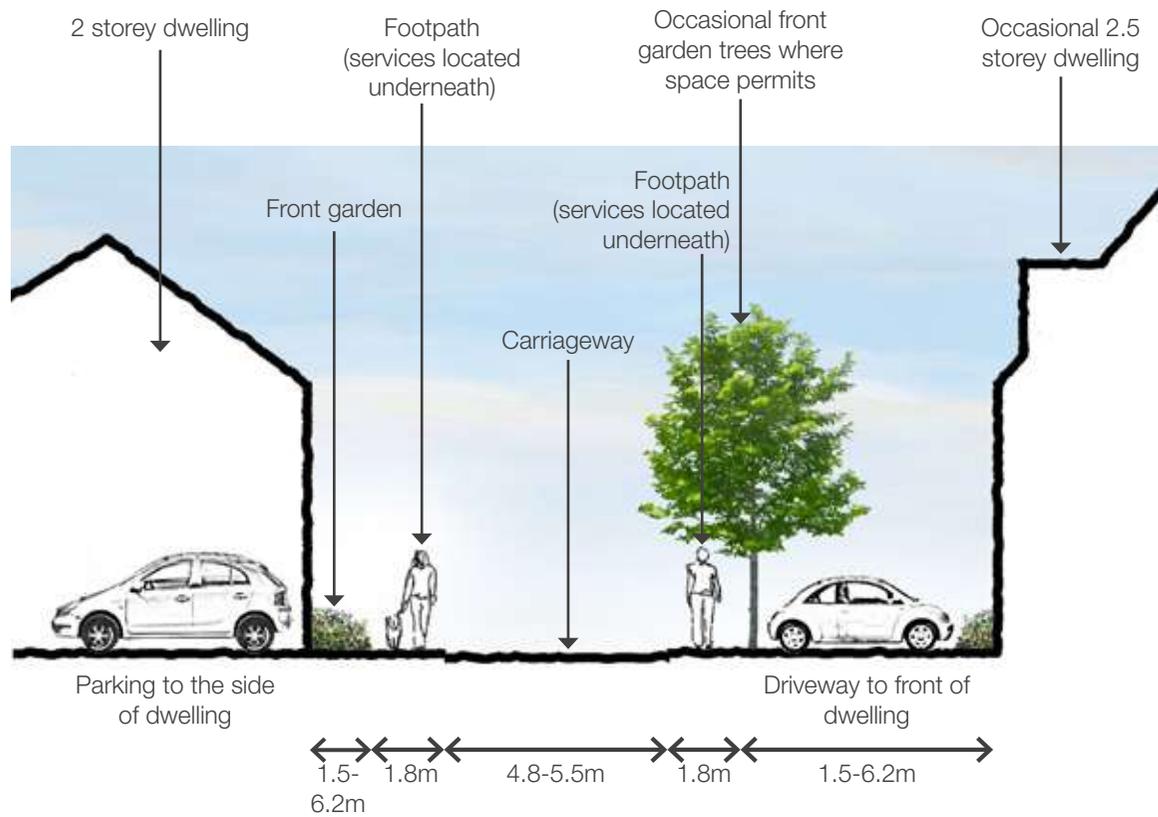
These are secondary connecting streets leading from the Green Spine and providing circular connections back to the Green Spine. The streets will be designed to maintain a sense of enclosure and to reduce vehicle speeds. Well-defined boundary treatments will help to distinguish between private and public space.

Development along these roads will generally comprise a higher density of development, with semi-detached properties, short terraces where appropriate and occasional detached properties.

### Key Design Principles:

- **Parking on plot with driveways and garages**
- **Tree planting positioned within larger front and side gardens to provide interest on the street scene**
- **1.8m footpath to both sides of the carriageway**
- **2 storey buildings with occasional 2.5 (& single storey where streets back on to the established settlement edge)**
- **More informal building line with a mix of building types, and deeper setbacks where required to accommodate frontage parking**
- **More informal in character compared to the Green Spine**
- **Focal buildings and trees**
- **Some buildings located with gable ends onto the street**
- **Detached dwellings located towards the edge of development parcels**

## Secondary Streets Indicative Section



## Secondary Streets



CODE 3.7	Secondary Streets	
Design Speed	20mph.	
Carriageway Width	4.8m - 5.5m depending on location and number of dwellings they serve.	
Footpath	1.8m on both sides.	
Cycleway	Within carriageway.	
Verge	None.	
Trees	Where space permits within private front and side gardens.	
Parking	Typically on plot parking within private driveways and garages. Parking to the side or front of dwellings.	
Buildings	Street Character Principles	A semi continuous building line. Properties to front the street, occasional gable ends to street corners. A mix of terrace, semi-detached and occasional detached properties.
	Dwelling Form	Narrow and some wide plan.
	Dwelling Height	Primarily 2 storeys with occasional 2.5 storey at focal points. Single storey where streets back on to single storey at the established settlement edge.
	Dwelling Materials	Shown within Design Code 6.1.
Boundary Treatment	Private frontages 1.5-6.2m (minimum) depth, defined by clipped hedgerows / planting areas within front gardens and walls where adjoining buildings or garages.	
Materials	Carriageway: Accord with NCC specification. Footpath: Accord with NCC specification.	
Services	Within highway corridor.	

## Lanes

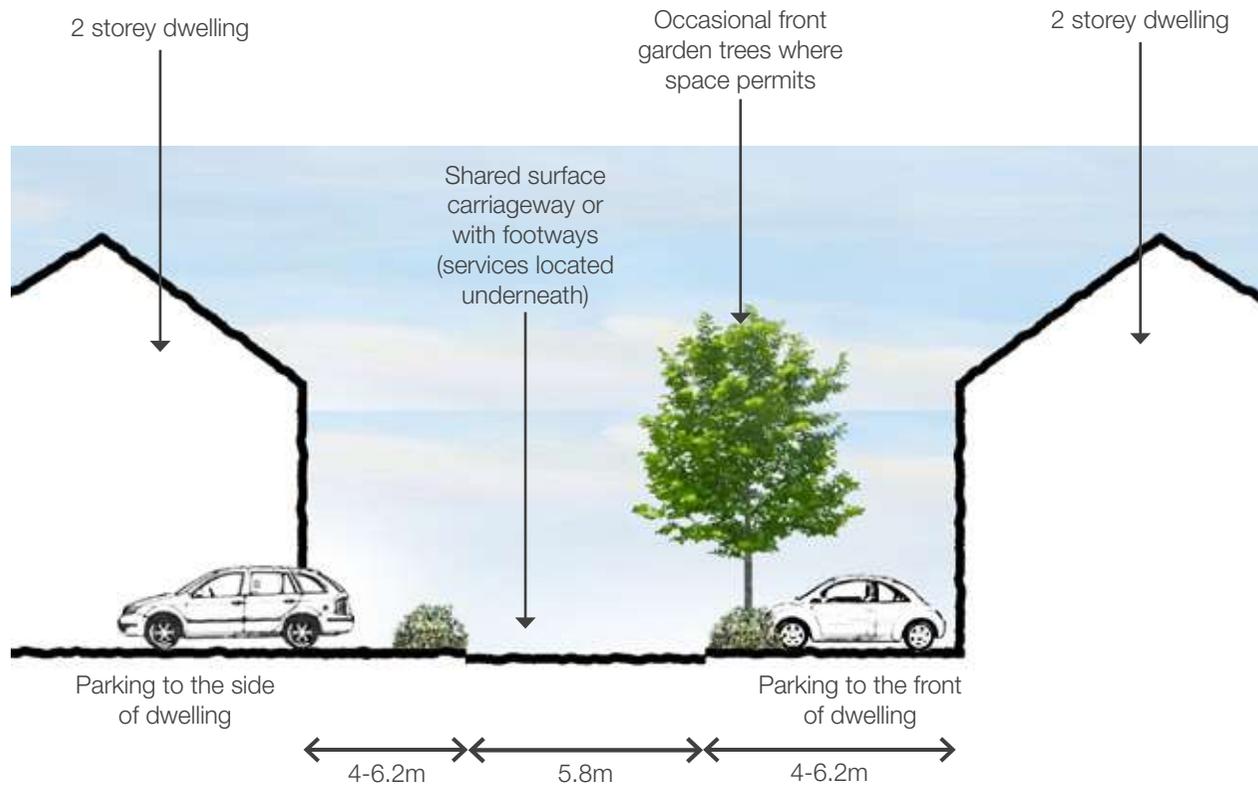
These routes are accessed from the primary and secondary streets. The street width will be restricted as much as possible to maintain a sense of enclosure and to reduce vehicle speeds. At various points within the development The Lanes will provide connections to the surrounding green space.

Housing types will be mixed, typically at 2 storey with varying densities to provide transition from the higher densities areas along the Green Spine to the lower density areas located towards the edge of the built development. Dwellings will predominantly be semi-detached and detached with occasional terraced or linked properties.

### Key Design Principles:

- **Parking on plot with driveways and garages, with some frontage parking**
- **Tree planting positioned within larger front and side gardens to provide interest on the street scene**
- **Shared surface treatment, or with footways**
- **2 storey buildings (& single storey where streets back on to single storey at the established settlement edge)**
- **Deeper private frontages but no formal boundary treatment**
- **Predominantly semi-detached and detached dwellings in a relatively informal arrangement, occasional terraced and linked properties**
- **Some buildings located with gable ends onto the street**
- **Detached dwellings located towards the edge of development parcels.**

## Lanes Indicative Section



Lanes



CODE 3.8		Lanes
Design Speed	15mph.	
Carriageway Width	Between 5.8m and 7.8m.	
Footpath	Varies.	
Cycleway	N/A on Carriageway.	
Verge	None.	
Trees	Where space permits within private front and side gardens. Occasionally within adoptable highway to act as traffic calming.	
Parking	Typically on plot parking within private driveways and garages. Parking to the side or front of dwellings.	
Buildings	Street Character Principles	Relatively informal arrangement of buildings (when compared to the primary & secondary streets). Properties to primarily front on to the street, some gable ends to street corners. Primarily a mix of semi-detached and detached properties.
	Dwelling Form	Predominantly wide plan with some narrow plan.
	Dwelling Height	2 storeys. Some single storey where lanes back on to the established settlement edge.
	Dwelling Materials	Shown within Design Code 6.1.
Boundary Treatment	Private frontages 4-6.2m (minimum) depth, no formal boundary treatment. Small planting areas within front gardens and walls where adjoining buildings or garages.	
Materials	Carriageway: Accord with NCC specification.	
Services	Within highway corridor.	

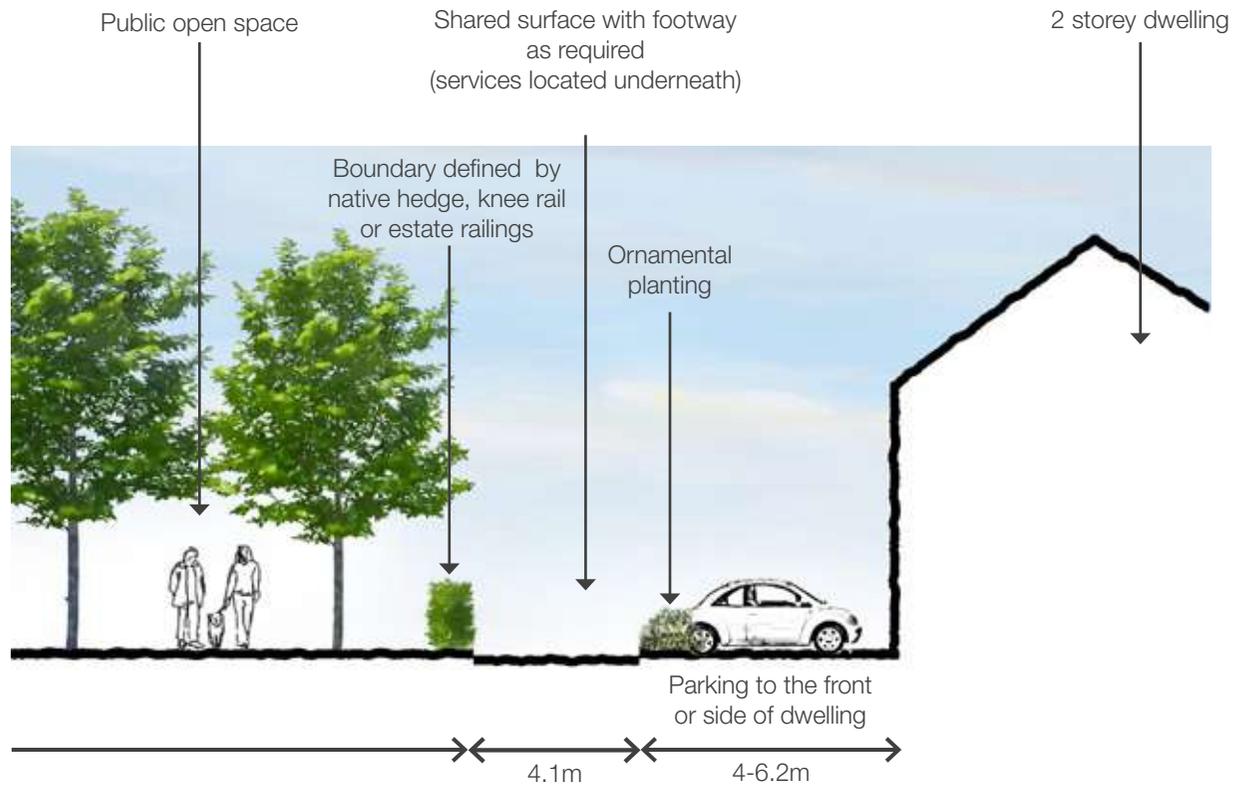
## Shared Private Drives

These routes are broadly restricted in use for up to nine dwellings that front onto open spaces and Greenways. The Shared Private Drives main purpose is to provide vehicle access for residents and services. As such these tertiary routes will typically be private in character, comprising either shared surface with no footways or a carriageway with footways either side.

### Key Design Principles:

- **Tertiary no-through route (vehicles)**
- **Provide connectivity to the Greenways and public open spaces that they front**
- **Private in character**
- **Shared surface treatment with footway as required, for permeability**
- **Parking on driveways and garages on plot**
- **Deeper private frontages**
- **Low density**
- **Predominantly detached and semi-detached dwelling**
- **2 storey**

## Shared Private Drives Indicative Section



Shared Private Drives



CODE 3.9		Shared Private Drives	
<b>Design Speed</b>	10-15mph.		
<b>Carriageway Width</b>	4.1m.		
<b>Footpath</b>	Within carriageway – shared surface or separate as required.		
<b>Cycleway</b>	None.		
<b>Verge</b>	None.		
<b>Trees</b>	Where space permits within private front and side gardens.		
<b>Parking</b>	On plot parking within private driveways and garages. Parking to the side or front of dwellings. Where parking bays are to the front an additional 1.9m depth is required by Highways.		
<b>Buildings</b>	Street Character Principles	Shared surface street. Informal arrangement of buildings overlooking green infrastructure. Properties to primarily front on to the street, some gable ends and angled frontages onto the street. Lower density arrangement with a higher percentage of detached properties and deeper frontages.	
	Dwelling Form	Predominantly wide plan with occasional narrow plan.	
	Dwelling Height	2 storeys.	
	Dwelling Materials	Shown within Design Code 6.1.	
<b>Boundary Treatment</b>	Private frontages 4-6.2m (minimum) depth, no formal boundary treatment. Turf with small planting areas within front gardens and walls where adjoining buildings or garages. Native hedgerow planting, knee rail or estate railings to outer edge of shared private drive to define boundary between the shared private drive and the public open space.		
<b>Materials</b>	Carriageway: Accord with NCC specification.		
<b>Services</b>	Within highway corridor.		



Examples of 'Village' House Types

## Character Areas

Three character areas are proposed across the development. These have been designed to reflect the nature of the settlement and its relationship to the surrounding rural context. Within the east and around the established settlement fringes a **'village character'** is proposed. To the west within proximity of St Peter's Church and to the south-west bordering the rural edge a **'rural character'** is proposed. Finally, centrally and within proximity of more recent development at the settlement edge a **'contemporary character'** is proposed. A palette of materials will be used to clearly define each of these character areas. Further detail is set out at Code 6.1.



Examples of 'Contemporary' House Types

Additionally, each character area will be subject to differing planting styles that will be unique to each character area. Further explanation is provided within **Section 4** and the detail for this is set out within the Landscape Framework Plan.



Examples of 'Rural' House Types



- Village
- Rural
- Contemporary

*Character Areas*

## Section 4

# BLUE/GREEN INFRASTRUCTURE

### Green Infrastructure Principles

The Green Infrastructure principles for Easton take a landscape-led approach. A high-quality Green Infrastructure (GI) is an essential component of the development. In total circa 16.6Ha of GI is proposed which equates to circa 38% of the total site area (this total includes the allotments, Four Acre Wood and ecological mitigation land).

The key GI principles are to establish a safe and enduring landscape that will provide a rich and diverse setting for the development, encourage healthy living activities including play, provide a distinct identity to the development, deliver greenspace enhancements for Easton as a whole, maximise biodiversity opportunities and deliver other sustainability benefits such as Sustainable Drainage (SuDS) and further encourage community food growing.

The existing site landscape of hedgerows, trees and woodland provides a robust framework to build upon. New hedgerows, trees, broadleaved woodland and orchards are proposed, along with areas of amenity, wildflower and

conservation grassland and varied wetland habitats. This approach will significantly enhance long term landscape character, habitat provision and connectivity. In addition, it will help to assimilate built development within the landscape and provide a considered transition from the existing and proposed urban realm to the rural landscape.

A landscape led approach draws the landscape into the development, knitting areas together and creating cohesion. A connected series of open spaces and Greenways allow the scheme to breathe and improve legibility.

The long-term success of the GI is addressed by way of the Ecological Impact Management Plan and the Green Infrastructure Management Plan. This document has been written to ensure that the site's Green Infrastructure, namely its network of public open spaces, play areas, footpaths, structural planting and habitats will be effectively managed to ensure their continued use for public amenity and to maintain and enhance the site's biodiversity.

The proposed blue/green infrastructure can also provide educational opportunities for residents and school children through the provision of wildlife areas and educational signboards within these areas.

The GI of the site is broken down into areas, each with distinct characteristics. These areas also inform the character of the built development that they adjoin. These areas are defined as 'Key Spaces' and described in detail in the following section (**section 5**). This section describes the overarching landscape Design Code principles for each of the key landscape components of the Landscape Framework Plan; soft landscape, play, allotments, SuDS, and ecological mitigation and landscape buffers.



- |   |   |  |  |                                    |
|---|---|--|--|------------------------------------|
| Site Boundary                           | Proposed Footpath (2m wide Macadam)             | Private Garden Trees   | Meadow Grassland                           | Hibernacula - indicative locations |
| Existing Tree/Tree Group Retained       | Proposed Footpath (2m wide Hard Binding Gravel) | Native Woodland Planting - Whips   | Rough/Long Tussocky Grassland              | Information Boards                 |
| Existing Hedgerow Group Retained        | Play Area                                       | Native Wetland Tree And Scrub Planting - Whips                                 | SuDS, Features including Swales and Basins |                                    |
| Existing Tree for Removal               | Allotments                                      | Native Hedgerow (POS areas)  | Proposed Timber Knee Rail                  |                                    |
| Existing Hedgerow for Removal           | POS Tree Planting                               | Hedgerow to Private Garden Frontages (key character area locations shown only) | Proposed Estate Railings                   |                                    |
| Existing Recreational Footpath Retained | Street, Parkland & Landmark Tree Planting       | Amenity Grass/Grass Verges   | Bat and Bird Box - indicative locations    |                                    |

**Landscape Framework Plan**  
 (Refer to Landscape Framework Plan 9033/L/02D Size A0 for further detail)

CODE 4.1	Green Infrastructure Generally
Layout / Character	Connected green spaces and streets, each with distinct characteristics, defined as 'Key Spaces'. A landscape buffer will be provided adjacent to the existing settlement edge.
Materials	Although each area will have its own design language, a consistent palette of materials and street furniture are to be applied throughout the landscape of the scheme to provide coherence. See Code 6.4.
Soft Landscape	Existing trees, hedgerows and woodland will be retained where possible and incorporated within the Green Infrastructure. A green infrastructure management plan will safeguard the areas of retained vegetation within the Site. A common base palette of planting is also used throughout the scheme (see Code 4.2). It provides a backbone of key species that provide a linkage between the different areas and design approaches. A broad range of locally occurring native species will be utilised within the Green Infrastructure areas, with scope for semi-ornamental trees and plants on the street scene.
Technical / Accessibility	Residential areas to be designed to promote accessibility for all and dementia friendly principles.  All surfaces are to not exceed 1:21. Continuous footpaths to be provided to slow vehicles and give pedestrians priority. Where not practicable drop kerbs to be used at road crossing points in conjunction with tactile paving. All bollards are to be fitted with visibility strips for the visually impaired. Any boardwalks and bridge crossings are to be designed to ensure that the deck is flush with adjacent paths.
Management and Maintenance	There will be a hierarchy of management adoption solutions, POS and SuDS features will have a different approach to management and maintenance.



## Ecological Mitigation

The site was previously surveyed in 2013-15. A new suite of protected species surveys has been undertaken to update the previous assessments and to inform the Design Code and future design work.

The principal protected species and habitat features within the site and their mitigation / enhancement prescriptions are indicated on the adjacent plan and within the table below. The project ecologist recommends the following locally occurring native planting species for the site:

Hedgerows: hawthorn (main species) with dogwood, elm, hazel, crab apple, hornbeam, holly, spindle, oak, cherry and guelder rose.

Trees: to include oak (subject to spatial constraints and ground conditions), hornbeam, cherry, cherry plum and field maple, planted in locations that ensure sufficient space for future growth of new trees.

Careful consideration will also be given to including planting to encourage pollinating insects in line with the emerging Norfolk Pollinator Strategy (likely to be published later this year). This includes ornamental planting within the main development area. The following structural planting species are proposed to provide a clear sequence of blossom of high value to pollinators (care to be taken to ensure species selected are suitable for their location):

- **Early:** cherry plum *Prunus cerasifera*, goat willow *Salix caprea*.
- **Mid-season:** hawthorn *Crataegus monogyna*, apples and pears including domesticated varieties, other willows, and wild cherry *Prunus avium*.
- **Late season:** field maple *Acer campestre*, dogwood *Cornus sanguinea*, elder *Sambucus nigra* and gorse *Ulex europaeus*.

Wildflower grassland is also proposed to provide further enhancement for pollinating insects.

Carefully placed dead wood and log piles will be incorporated within informal GI around the edges of the site, in sunny and shady locations, to provide hibernacular for bees, great crested newts and slow worms.

The scheme will include measures to ensure it is permeable to hedgehogs – holes in base of fences / gravel boards to allow hedgehogs to pass through and gates raised to 15cm above the ground to allow hedgehogs to pass under.

A bat, house sparrow, house martin or swift box may be fitted to appropriately located dwellings.

CODE 4.2	Ecological Mitigation (refer to plan)
<b>A. Important Hedgerow (in accordance with the Hedgerow Regs 1997)</b>	This will be retained as the boundary between the development edge and the new allotments. A single gap will be created to provide vehicle access to the allotments from the development.
<b>B. Hedgerow</b>	Limit impacts on the very north-west and eastern 1/3 of this hedgerow.
<b>C. GCN Mitigation Area</b>	Off-site land to be managed grassland and incorporate small areas of scrub planting and hibernacular to provide for GCN mitigation. The greenspace to form continuous tracts of habitat with adjoining areas.
<b>D. Slow Worms</b>	To be translocated to area C.
<b>E. Bat Commuting</b>	Within the north-west of the site, the northern boundary trees, vegetation around the Diocese, and an east-west hedgerow between the Diocese and western boundary hedgerow will be maintained as bat corridors for bat commuting. Breaks in vegetation to create access will be minimised and where any access is proposed a 'hop-over' will be created by planting a tree to each side of the access. Minimise lighting to commuting corridors and maintain dark edges to the site. Supplementary tree planting will provide further enhancement (and will contribute to the Green Infrastructure Corridor identified in Local Plans). Hedgerows will be retained in the public realm for management purposes.
<b>F. Landscape Corridor</b>	This linear feature is currently a grassed bund. The bund is off-site. However the base (on-site within private gardens) will be planted with a native hedgerow and trees to provide a soft green landscape edge to the development and to create a linear green feature that will enhance north-south movements for wildlife.





(Refer to Code 4.2)  
Ecology Mitigation Plan

## Soft Landscape

The development will provide a range of open spaces and green routes to be enjoyed by people of all ages and abilities. The landscaping will comprise formal green and urban spaces and streets, to informal, naturalistic green spaces and greenways that reflect the rural character and provide a suitable transition to the rural context of Easton.

The majority of planting within the scheme will focus on native species of trees and plants, particularly throughout the peripheral transitional spaces at the urban-rural interface. A list of suitable locally-occurring native species to be included within the scheme has been prepared by Hopkins Ecology. Non-native planting is also appropriate within more formal spaces and along streets. The choice of tree species will be subject to locational factors including proximity to utilities, services, building foundations, and the highway and paved areas. Larger growing specimens should be specified wherever space permits, such as some sections of the Green Spine and within public open space areas.

Street trees will be planted along the Green Spine and within focal squares and spaces throughout the development.

Elsewhere trees within tertiary streets will be provided at focal points within private front and side gardens. The texture, form, size and seasonal interest should be key design considerations for the selection of tree species. Use of particular species in key locations will help to reinforce a sense of place and the individual character of each of the 'Key Spaces' and Character Areas. For instance, as the Green Spine passes through rural, contemporary and village character areas, the location and species of street trees and frontage hedgerows will change in accordance with the character area. The landscaped character of the swale will also alter for each character area. The hedgerow, tree and swale landscaping principles are shown on the Landscape Framework Plan.

Planting across the scheme will be implemented at the appropriate size to provide instant visual benefits and / or to maximise the opportunity for successful establishment. All standard trees will be either rootball or container grown, dependant on time of planting and supply.

Native hedgerow and woodland planting will be specified as bare root and planted at a density of 5/linear metre (hedgerows) and 1.5m centres (woodland planting) and protected with tree/shrub rabbit guards. This planting must be undertaken during the winter months.

Formal spaces will include some non-native plant specimens. A selection of suitably robust species that provide seasonal interest throughout the year are recommended in the Design Code list below. These plants will also be included within the planting schemes for private front gardens, which will also include a proportion of flowering herbaceous perennials to provide food for invertebrates, including bees and butterflies. Detailed on-plot planting plans will be prepared to support the reserved matters for each phase of the development.

All trees within the public realm, including verges will be maintained and transferred to a management company or highways authority as appropriate. Maintenance will accord with BS 8545 - clause 11.1-11.5 - Post-planting management and maintenance.

**CODE 4.3**      **Examples of Typical Plant Species: Street/focal trees, Native trees and shrubs, Formal shrubs and perennials (list is not exhaustive)**

*Native Shrubs and Trees*



1. Dogwood; 2. Hawthorn; 3. Guelder rose; 4. Crab apple; 5. Wild cherry; 6. Goat willow; 7. Oak;  
(And other native species, refer to page 59)

*Street, Parkland and Landmark Trees*



8. Acer campestre 'Green Column'; 9. Betula utilis 'Jacquemontii'; 10. Cercis siliquastrum; 11. Liquidambar Styraciflua 'Worplesdon'; 12. Prunus sargentii; 13. Ulmus 'Columella'; 14. Beech  
Tree species selection to be appropriate for the space available; sections of the Green Spine and much of the POS will be suitable for (ultimately) large trees.

*Garden Planting (hedges, shrubs, perennials and small trees)*



15. Prunus laurocerasus 'Otto Luyken'; 16. Lavandula 'Hidcote'; 17. Hornbeam (hedge); 18. Geranium 'Johnsons Blue'; 19. Astrantia major; 20. Philadelphus 'Belle Étoile'; 21. Prunus 'Accolade'  
(Planting species selection to include a high percentage of plants that provide food for pollinating insects, including bees)

## Play Spaces

The proposed development provides a significant amount of open space for natural play, informal sports and recreation, including the provision of equipped play areas.

The central Easton Green will provide the primary play and informal sports provision and will include a Neighbourhood Equipped Area of Play (NEAP). A further two Locally Equipped Areas of Play (LEAP) are proposed, one located within the eastern neighbourhood (Home Green) and one within the west (Four Acre Green). A further eight Local Areas of Play (LAP) are distributed throughout the development, providing for doorstep play for younger children. Play space details to be agreed at each relevant reserved matters stage.

The following requirements are set by Fields in Trust for play facilities and will be adhered to with the proposed scheme:



CODE 4.4		Play Area Requirements
LAPS	Aimed at children up to 6 years of age. Will include; a minimum activity zone of 100m <sup>2</sup> ; 5m minimum separation between activity zone and the boundary of dwellings; and hoop top fencing of minimum 600mm high with a self-closing pedestrian gate.	
LEAPS	Aimed at children up to 8 years of age. Will include; a minimum activity zone of 400m <sup>2</sup> ; 20m minimum separation between activity zone and the habitable room façade of dwellings; play equipment designed to stimulate balancing, rocking, climbing, sliding, swinging, jumping, crawling, rotating, counting, touching and imaginative play; has adequate space around the equipment to allow for games; will provide cycle parking and a litter bin.	
NEAPS	Aimed at older children. Will include; a minimum activity zone of 1000m <sup>2</sup> ; 30m minimum separation between activity zone and the boundary of dwellings; comprising an area for play equipment and structures and a hard-surfaced area of at least 465m <sup>2</sup> (the minimum needed to play five-a-side football); play equipment that will include challenging equipment aimed at adventure and being active; will provide cycle parking and a litter bin.	
All Play Facilities (Designed to be 'Accessible by Design')	Layout	Will be evenly distributed to provide ease of access and minimise walking distances to a play facility for residents. Will be located along pedestrian and cycle routes and form a part of the connected series of 'Key Spaces'. Will be overlooked by nearby homes and from circulatory routes and streets.
	Character / Materials	The character of the play areas and equipment will be designed to reflect the characteristics of the Key Space in which it is located, providing for a range of formal and informal play spaces. Will provide seating and signage indicating that dogs are excluded. Will incorporate grass and/or hard surface with appropriate impact-absorbing surfaces for the equipment specified.
	Planting	Play areas will be positioned within amenity grassland however areas of conservation grassland and native (or where appropriate semi-ornamental) shrub and tree planting will be incorporated as part of a comprehensive layout design. The planting will add interest and help to soften and blend the play areas within the Greenspace. For safety, care will be taken in the planting design to ensure an open aspect that will allow natural surveillance of the area to continue as planting matures.

## Allotments

The existing allotments will be relocated to a larger site (1.407ha) adjoining the western edge of the development and will be refitted for the benefit of new and existing residents of Easton. The new allotments will be made available to residents prior to the closure of the existing allotments to allow for a smooth transition between land holdings for current allotment holders. The following design principles will be adhered to;

CODE 4.5	Allotments
<b>Layout</b>	To be orientated north-south. To include a securely gated car park (1 space per 10 standard size plots). Double gates to be 3m wide. Direct access from the carpark / gate to a main path (up to 2m width) through the allotments which leads to arterial paths (up to 1m width) that ensure easy access to all plots. Standard plot size 10x25m, half plots could be made available if desired by residents. Provide a central community hub (4x4m shelter with composting facility).
<b>Materials</b>	To include boundary green mesh security fencing (1.8m high) if required in combination with native hedgerow. Internally, post and 3 wire fencing will sub-divide plots. One shed (8x6ft) per plot. All sheds to be of consistent design and style. The car park and paths will be timber edged and a crushed stone surface dressing used throughout.
<b>Services</b>	To include a suitable water supply and an access road which will link the new allotments to the adopted public highway network. The access road will be tarmac surface. Water taps to be shared use (typical provision – 1 tap per 10 gardens). Provide a central community hub (4x4m shelter with composting facility).



## Sustainable Drainage Systems (SuDS)

SuDS will attenuate surface water runoff and discharge that arises from the development. The SuDS include, but are not limited to, the creation of 'detention' and infiltration basins, a network of swales, with potentially also filter drains and rain gardens where appropriate.

A series of detention and infiltration basins will be located to the low points within the Site, within the areas of Greenspace, including the Gateway adjacent to Dereham Road. The 'basins' themselves, will create a strong landscape feature. Along the primary street ('The Green Spine') swale features have been designed to connect with the 'detention basins'. The distribution of SuDS features is designed to mimic the greenfield environment, with more frequent and smaller basins distributed throughout the development to allow rainfall to be dealt with at source and thus provide more resilience.

As well as providing a drainage function, the basins and swales form an important part of the development's Green Infrastructure strategy. These features will be designed so that they create opportunities for habitat creation and wildlife. This includes a connected network of features, a range of slope angles, including shallower slopes, low density native planting suitable for the conditions, avoiding overly shading the south side of SuDS areas, and ecological grassland management that allows areas of longer and tussocky grass to form within and around the SuDS areas.

In addition, where appropriate, the SuDS will be designed as dual use areas, with the potential to be used for public open space where they would only be wet in extreme events for a few days per year. This will be determined on a case by case basis within the masterplan design. During construction care will be taken to retain existing soil structures within open space areas not required for construction operations.





- Swales
- SuDS Basins

*SuDS Plan*

CODE 4.6	SuDS
<b>Layout</b>	Basin and swale profiles to be constructed to be natural in appearance and as an integral component of green space.
<b>Character</b>	<p>SUDS basins and swales will be designed to provide a positive landscape and ecological feature whilst satisfying sustainable drainage requirements.</p> <p>Swale designs will vary depending on ground levels, topography, soil conditions, orientation, aspect and proximity to other landscape features and buildings. Swales should have an appropriate scale and form to suit the surrounding landscape character. The detailed design will reflect the character area in which the swale is located as defined by the Landscape Framework Plan.</p>
<b>Planting</b>	<p>Native grassland will be sown within basins and swales. Native wetland planting will be included in appropriate locations; carefully positioned to ensure it retains an open aspect and doesn't impede natural surveillance as it matures. Native wetland specimen tree species will be planted individually and in small groups for impact.</p> <p>SuDS detention basins, where identified, will form part of the informal open space used for recreation. These areas will only be wet in extreme events for just a few days per year. The management regime will ensure areas of shorter grassland are maintained within these basins that are suitable for amenity access.</p>



## Section 5

# KEY SPACES

### Introduction

This section of the Design Code addresses the requirements for each of the Key Spaces identified on the opposite plan, some spaces will be similar in character and therefore the descriptions are grouped as follows:

#### 1. Gateways:

- 1a. **Easton Gateway** – forms the primary entrance to the development from Dereham Road, and also the new Gateway to the settlement on the approach from the east.
- 1b. **Dereham Road** – north of Dereham Road the new residential frontage to the street will be consistent, locally characteristic and of a high-design quality.
- 1c. **Church Green** – the western entrance to the development will be carefully designed to respond to its sensitive heritage and ecological context.

2. **Green Spine:** the primary route through the development and will provide a combined green / SuDS corridor that all of the Key Spaces connect in to.

3. **Easton Green:** the central primary greenspace for the development, with adjoining community facilities, will serve the entire Easton community and will contain a NEAP.

#### 4. Neighbourhood Greens:

- 4a. **Home Green** – the central green serving the eastern neighbourhood (with LEAP).
- 4b. **Deer Park Green** – close to Easton Green, this smaller green serves the central neighbourhood (with LAP).
- 4c. **Broom Lane Green** – serves the south-western neighbourhood (with LEAP).
- 4d. **Four Acre Green** – serves the western neighbourhood (with LEAP).

5. **Greenways:** green corridors provide a transitional landscape between the development edge and the surrounding rural landscape and include network of recreational footpaths and connections.

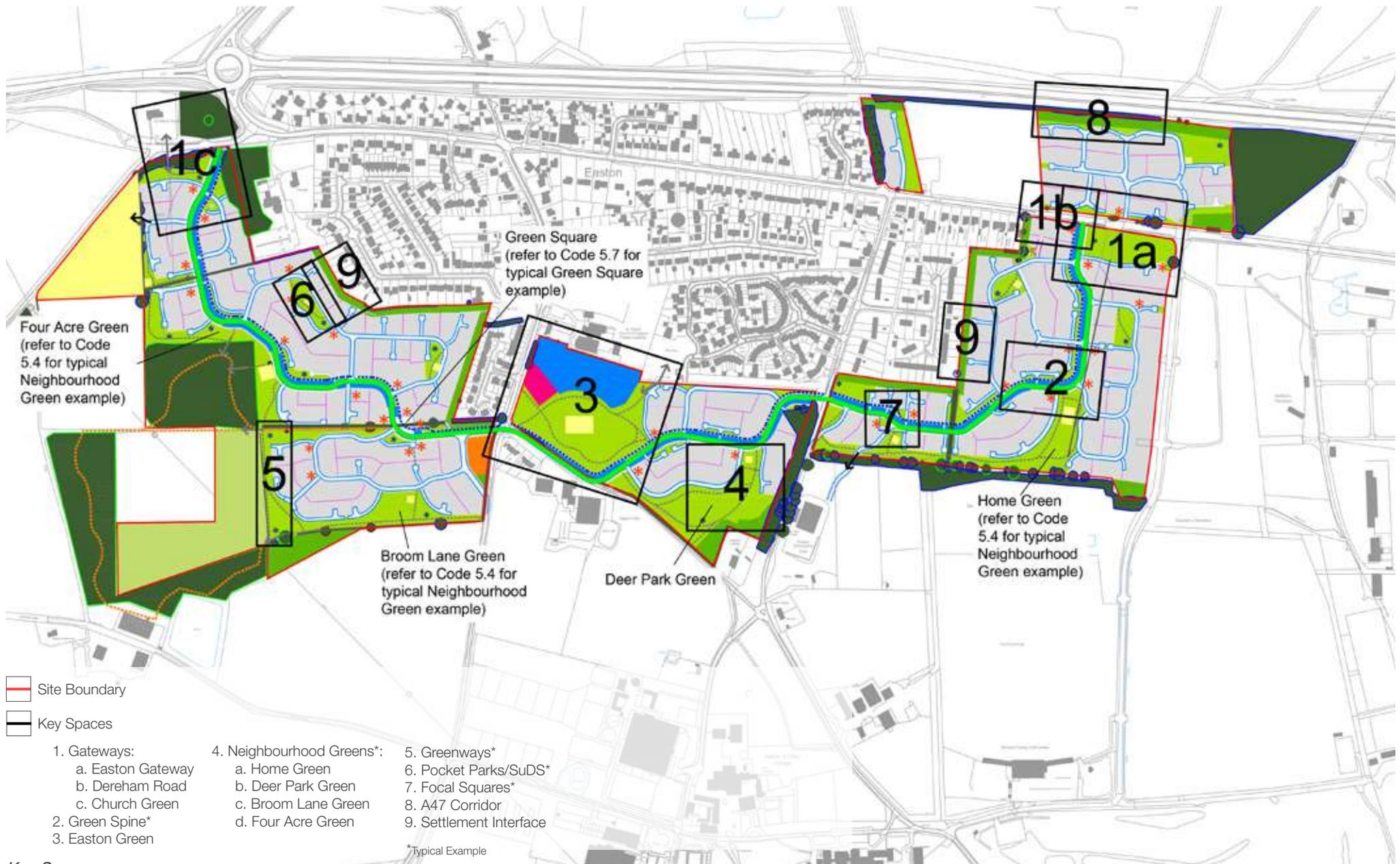
6. **Pocket Parks:** small greenspaces within the built development area, contain LAPS and / or SuDs features.

7. **Focal Squares:** two focal spaces provide arrival points along the Green Spine route.

8. **A47 Corridor:** a green noise and landscape buffer between the A47 and the northern edge of the development.

9. **Settlement Interface:** an appropriate green buffer is proposed between the existing settlement edge and the development.

The description and Design Code for each of the Key Spaces is described through text, tables and illustrative sketches and photos on the following pages.



**Key Spaces**

- 1. Gateways:
  - a. Easton Gateway
  - b. Dereham Road
  - c. Church Green
- 2. Green Spine\*
- 3. Easton Green
- 4. Neighbourhood Greens\*:
  - a. Home Green
  - b. Deer Park Green
  - c. Broom Lane Green
  - d. Four Acre Green
- 5. Greenways\*
- 6. Pocket Parks/SuDS\*
- 7. Focal Squares\*
- 8. A47 Corridor
- 9. Settlement Interface

\*Typical Example

## 1a. Easton Gateway

This Key Space forms the primary entrance to the development from Dereham Road, and also the new Gateway to Easton on the approach from the east. The character of the space is primarily defined by the proposed semi-formal green space that will incorporate a SuDS facility. A grassed SuDS detention basin is proposed that will be primarily dry, with gently sloping sides to allow for multifunctional use. Landscape planting will include native species and larger- growing trees to provide visual impact. A semi-formal, low density arrangement of 2-storey detached or semi-detached dwellings will front the space.

### Key Design Principles:

- **Easton gateway**
- **Semi-formal character**
- **Strong building frontage**
- **Landscaped green space with SuDS**
- **Properties to front focal greenspace**
- **Low-density arrangement**
- **Detached or semi-detached dwellings**
- **Shared Private Drives**



*Key Plan (refer to Key Spaces Plan p.71)*



CODE 5.1a	Easton Gateway
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	Dwellings set behind green space to create gateway entrance from the eastern Dereham Road. Informal, low-density arrangement of buildings with low-medium degree of enclosure between buildings to create variety and avoid a hard edge.
<b>Building Frontage</b>	Dwellings to be orientated to face the greenspace. Corner properties to be dual aspect to address the green space and street.
<b>Green Space and Planting</b>	Greenspace to comprise informal POS with SuDS feature. Semi-natural character, predominantly utilises native locally occurring plant species, street trees may be non-native. Selective use of larger specimens will provide instant visual benefit. SUDs feature to be deeper and planted close to the green spine road, becoming shallower to the east to form part of the POS. A balance of mown grass and longer grassland for amenity and wildlife.
<b>Design Parameters</b>	
<b>Building Type</b>	Typically detached with some semi-detached.
<b>Building Height</b>	2-storey.
<b>Set Back</b>	Variation in set-back to accommodate deeper front gardens of up to 6.2 metres (minimum) from edge of highway / private drive.
<b>Roof Form</b>	Gable pitched roof, generally running parallel to the street. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.
<b>Boundary Treatment</b>	Dwelling frontages enclosed with mixed ornamental shrub planting with some clipped hedgerows. Any visible side boundaries to be 1800mm high brick garden walls. POS to be bordered by native hedgerow or estate railings.
<b>Streets and Parking</b>	Access may be directly from the green spine or via a lane or shared private drive. Private drives with parking to the front and / or side of properties. Garages to the rear or side.
<b>Materials</b>	The Easton Gateway falls within the 'Village' Character Area as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.

## 1a. Easton Gateway



- 1 Landscaped attenuation
- 2 Meadow grass
- 3 Native tree and shrub planting
- 4 Dwellings front onto open space
- 5 Landscaped swale
- 6 Combined pedestrian and cycle path
- 7 Estate railings
- 8 Dual aspect gateway property
- 9 Hedgerow along Dereham Road to be retained
- 10 Focal tree



1a. Easton Gateway Visualisation

## 1b. Dereham Road

Fronting Dereham Road the new residential frontage to the street will be consistent, locally characteristic and of a high-design quality. New dwellings will be set back beyond a green frontage that incorporates a grass verge, hedgerow and individual trees and north of the road will be accessed via shared private drives that will limit the number of new access points directly onto Dereham Road. This is a precedent set by recent development elsewhere along the north side of the main street through Easton, with direct access to private drives on the southern side.

### **Key Design Principles:**

- **Semi-formal Character**
- **Verge and hedgerow frontage to the street**
- **Buildings set back from street**
- **Detached or semi-detached dwellings**
- **Buildings to wrap around site frontage onto Green Spine to create entrance feature**



*Key Plan (refer to Key Spaces Plan p.71)*



Dereham Road Precedents

CODE 5.1b		Dereham Road Gateway
<b>Urban Form</b>		
<b>Spatial Arrangement</b>	Buildings set back from the street beyond grass verge, hedgerow. Formal low-density arrangement of buildings with a medium degree of enclosure between buildings.	
<b>Building Frontage</b>	Dwellings to be primarily orientated to face the street. Corner properties to be dual aspect to address Dereham Road and any side streets.	
<b>Green Space and Planting</b>	Grass verge to back of footpath with continuous native single species hedgerow to street, with gaps to access individual properties. Informal spacing of hedgerow trees (minimum 3No per 50 metre) may be non-native.	
<b>Design Parameters</b>		
<b>Building Type</b>	Typically detached with some semi-detached.	
<b>Building Height</b>	2 storeys to reflect existing character of Dereham Road.	
<b>Set Back</b>	Variation in set-back from 4- 6.2 metres (minimum) from back of highway.	
<b>Roof Form</b>	Gable pitched roof, generally running parallel to the street. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.	
<b>Boundary Treatment</b>	Front boundary defined by a hedge. Ornamental planting in focal locations adjacent to dwelling frontages/path access to properties.	
<b>Streets and Parking</b>	Access directly from Dereham Road and green spine. Private drives with parking to the side of properties. Garages to the rear or side.	
<b>Materials</b>	Dereham Road falls within the 'Village' Character Area as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.	

## 1b. Dereham Road



- 1 Dual aspect gateway property
- 2 Dwellings set back from Dereham Road
- 3 Dwellings set back from Dereham Road, and accessed by shared private drives
- 4 Hedgerow retained and supplemented with tree planting
- 5 New hedgerow and tree planting with grass verges
- 6 Focal tree
- 7 Landscaped attenuation
- 8 Landscaped swale
- 9 Planted buffer



1b. Dereham Road Visualisation

## 1c. Church Green

Careful design is required for this area in order to respect and enhance the setting of the grade I listed church and the Diocese building, and to retain and enhance bat habitat and foraging corridors. The existing mature landscape vegetation is to be retained and the allotments will be relocated to the adjacent field to the west. A new parkland area is to be created to provide an attractive western gateway to the development, protect the setting to the church and the Diocese building, and provide further habitat for bats. The position of the Green Spine will be carefully located to minimise impacts on existing vegetation and new dwellings will be set back within the site in a low density informal arrangement, with careful use of materials appropriate to the local context. Further consideration will be given at the detailed design stage to the relationship between the church, access and the development.

### Key Design Principles:

- **Treatment of this area is designed to reflect character of the setting of the Diocese building and the church**
- **Development gateway**
- **Semi-formal parkland character predominates**
- **Landscaped green space to protect setting to Church**
- **Properties to front greenspace**
- **Informal very low-density arrangement**
- **Predominantly large detached and occasional semi-detached dwellings**
- **Buildings set back from greenspace beyond a shared private drive and hedgerow**



*Key Plan (refer to Key Spaces Plan p.71)*



St Peter's Church



Trees between the Church and Site



Proposed Parkland Character

CODE 5.1c	Church Green Gateway
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	Buildings set back behind wide swathe of greenspace to comprise parkland. Informal, low-density arrangement of buildings with low degree of enclosure between buildings to create variety and avoid a hard edge.
<b>Building Frontage</b>	Dwellings to be primarily orientated to face the greenspace. Corner properties to be dual aspect to address the greenspace and street.
<b>Green Space and Planting</b>	Greenspace to include retained tree belt to front of the church and new planting of individual parkland style trees arranged informally within grass to provide an attractive entrance into the development. Selective management of sycamores along northern site boundary as they decline over time to develop a softer parkland setting to the church. Selective use of larger tree specimens will provide instant visual benefit.
<b>Design Parameters</b>	
<b>Building Type</b>	Predominantly detached, occasional semi-detached.
<b>Building Height</b>	2 storeys.
<b>Set Back</b>	Variation in set-back to accommodate front gardens of up to 1.5-4 metres from edge of shared private drives.
<b>Roof Form</b>	Gable pitched roof, generally running parallel to the shared private drive.. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.
<b>Boundary Treatment</b>	Dwelling frontages enclosed with clipped hedgerows. Estate railings to periphery of adjacent POS. Any visible side boundaries to be 1800mm high brick garden walls.
<b>Streets and Parking</b>	Access via shared private drive. Private drives with parking predominantly to the side of properties. Garages to the rear or side.
<b>Materials</b>	Church Green falls within the 'Rural' Character Area as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.

## 1c. Church Green



- 1 Parkland style planting
- 2 Landscaped attenuation
- 3 Dwellings front onto open space
- 4 Dual aspect gateway property
- 5 Estate Railings
- 6 St Peter's Church
- 7 Landscaped swale
- 8 Combined pedestrian and cycle path
- 9 Indicative footpath route (to be considered further at Reserved Matters stage)



1c. Church Green Visualisation

## 2. Green Spine

The primary route through the development will be an attractive green route set within a 19.5-metre wide corridor. A swale is proposed primarily to the north side of the road and in combination with grass verges and tree planting will clearly define the route through the development. Whilst the route will be contemporary in character the overarching built-form will comprise a formal arrangement of dwellings that directly address the adjacent green verge. Adjacent to the swale will be less formal, incorporating shared private drive access crossings over the swale.



Key Plan (refer to Key Spaces Plan p.71)

### Key Design Principles:

- **Primary route through the development**
- **Broad green corridor**
- **Incorporates SuDS features such as swales**
- **Tree planting on the street scene**
- **Dwellings to predominantly address the street**
- **Formal arrangement of dwellings opposite the swale with direct driveway access**
- **A semi-formal arrangement of dwellings adjacent to the swale with access via shared private drives**
- **Strong building frontage**
- **A range of dwelling forms from detached to terraced**

CODE 5.2	Green Spine
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	<p>A broad, green residential street, with a grass verge, swale and trees that clearly define it as the primary route.</p> <p>A medium to high density arrangement of dwellings that directly address the street with a high degree of enclosure and direct access to private drives and a medium level of enclosure and access via shared private drives across the swale.</p>
<b>Building Frontage</b>	<p>Dwellings to be primarily orientated to face the street.</p> <p>Corner properties to be dual aspect to address any side streets.</p>
<b>Green Space and Planting</b>	<p>Generally 8m of the Green Spine will be verge or swale.</p> <p>Trees to be distributed within POS and verges (avoiding highway verges and swales where possible) at an average distribution rate of 6 trees per 50 linear metres. Tree planting along the Green Spine route will be designed to be of a locally distinctive and unified character. Generally along the route medium-sized, tree species with an ultimate height of 12-17m will be proposed and where space permits, (e.g. alongside Easton Green) trees with a broader canopy and an ultimate height of 17-20m+ will be proposed.</p>

CODE 5.2	Green Spine
<b>Design Parameters</b>	
<b>Green Space and Planting (Continued)</b>	<p>A palette of core species used throughout will be supplemented with feature species reflective of the 'Rural' (e.g. native species), 'Village' (e.g. flowering species) and 'Contemporary' (e.g. architectural species) character areas.</p> <p>Trees may be non-native.</p> <p>Selective use of larger specimens will provide instant visual benefit.</p> <p>Swales to be mown grass or planted as appropriate.</p>
<b>Building Type</b>	A range of building types from detached to semi-detached and terraced (maximum of three in any one block).
<b>Building Height</b>	Up to 2.5 storeys.
<b>Set Back</b>	<p>Opposite the swale the buildings are set-back from back of highway footpath between 1m and 3m to create enclosure, and up to 6.2m (minimum) where frontage parking is proposed.</p> <p>Adjacent to the swale variation in set-back from 1.5m to 6.2m (minimum) from back of shared private drive depending upon parking arrangement.</p>
<b>Roof Form</b>	<p>Gable pitched roof, generally running parallel to the street. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.</p> <p>Occasional flat roof dormers.</p>
<b>Boundary Treatment</b>	Ornamental planting to dwelling frontages. Hedgerow planting to verges to be native mixed or single species dependant upon the character areas in which it is located.
<b>Streets and Parking</b>	<p>Varies:</p> <p>On plot parking within private driveways and garages, positioned opposite the swale and served off shared private drives adjacent to the swale.</p> <p>Frontage parking for continuous frontage properties adjacent to the swale to be limited, 90-degree, maximum 4 spaces per group with soft landscaping between groups.</p> <p>Parallel parking adjacent to green spaces, maximum 4 spaces per group with soft landscaping between groups.</p>
<b>Materials</b>	The Green Spine transitions through 'Village', 'Contemporary' and 'Rural' Character Areas as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.

## 2. Green Spine



- 1 Properties address The Street
- 2 Access via shared private drives adjacent to swale
- 3 Hedgerow and formal tree planting
- 4 Timber knee rail
- 5 Green space connection
- 6 Combined pedestrian and cycle path to Green Spine
- 7 LEAP
- 8 Landscaped swale



*Typical swale crossing example*



2. Green Spine Visualisation

### 3. Easton Green

The central primary greenspace for the development, with adjoining community facilities, will serve the entire Easton community. Peripheral land uses will be located so that they relate to the space. Accessibility for all is a key consideration. Parking will be carefully located to avoid dominating the space. The space will be designed to provide play and recreational opportunities for all ages and abilities, including provision of a Neighbourhood Equipped Area of Play (NEAP).



Key Plan (refer to Key Spaces Plan p.71)

The design approach is for the northern part of this area, including the school extension, shop and village hall (to be provided by others) to be of a contemporary design character. Within the south of this area, approaching the rural edge, where development fronts onto Deer Park Green and the countryside the design will take a more rural character.

#### Key Design Principles:

- Central large focal greenspace linked visually to the Green Spine
- Formal in character
- Design to address borders with the school extension land, village hall, existing properties, shop, Green Spine and new dwellings
- Large equipped play space
- Seating
- The school extension, village hall and shop (to be provided by others) should be of contemporary design and materials should complement the 'Contemporary' materials palette (defined within Section 6).

CODE 5.3	Easton Green
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	<p>Primary, focal greenspace with formal character. To incorporate a large play facility. Space to be designed to integrate and link surrounding land uses.</p> <p>New dwellings to be formally arranged to frame the space and be of medium – high density.</p>
<b>Building Frontage</b>	<p>Dwellings to be primarily orientated to face the greenspace.</p> <p>Corner properties to be dual aspect to address the greenspace and street.</p> <p>Visual links to be provided between the greenspace and the shop.</p>
<b>Green Space and Planting</b>	<p>Formal amenity grass and large-growing avenue trees to provide large-scale space for informal play. Local areas of semi-formal planting to assimilate play spaces and at edges of space to integrate internal and surrounding land uses.</p> <p>Play area to be of similar scale to a NEAP and designed to include a minimum of 8 pieces of equipment suitable for a range of ages.</p>

CODE 5.3	Easton Green
<b>Design Parameters</b>	
<b>Building Type</b>	Dwellings to be a range of building types from detached to semi-detached and terraced. Design of village hall and shop by others to be approved by South Norfolk Council.
<b>Building Height</b>	Up to 2.5 storeys.
<b>Set Back</b>	Building set-back from edge of highway/shared private drive between 1.5m - 3m to create enclosure, and up to 6.2m (minimum) where frontage parking is proposed..
<b>Roof Form</b>	Gable pitched roof, generally running parallel to the street. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.  Occasional pitch and flat roof dormers.
<b>Boundary Treatment</b>	Ornamental planting or lawns to dwelling frontages. Corner plot gardens to be edged with single species hedgerow planting.  Boundary of the Green to be defined with trees, hedgerow planting, and/or timber knee rails as appropriate.  The play facilities may be fenced with bow top fencing.
<b>Streets and Parking</b>	Village hall and shop parking to be detailed by others and agreed with South Norfolk Council.  Parking for dwellings to be predominantly private drives with parking predominantly to the side of properties and some to the front accessed directly from lanes or shared private drives.  Garages, where proposed are to the rear or side.
<b>Materials</b>	Residential areas on the periphery of Easton Green are proposed as 'Contemporary' Character Areas as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area. The village hall, shop and school extension to be contemporary constructed in contemporary materials that reflect/compliment those proposed for the dwellings.

### 3. Easton Green



- 1 School extension land
- 2 Village Hall
- 3 NEAP
- 4 Meadow grass
- 5 Amenity space
- 6 Combined pedestrian and cycle path
- 7 Swale adjacent to Green Spine
- 8 New native hedgerow and tree planting to replace the hedgerow and trees removed alongside Hall Road to accommodate the Green Spine





3. Easton Green Visualisation

## 4. Neighbourhood Greens

Across the development a further four green spaces are proposed – Home Green, Deer Park Green, Broom Lane Green and Four Acre Green. These link to the green spine and / or peripheral greenways and locally provide focal spaces to serve the eastern, southern and western neighbourhoods. Peripheral dwellings will be designed to address and enclose the spaces. Whilst the character of each space will vary slightly the same key design principles apply.



Key Plan (refer to Key Spaces Plan p.71)

### Key Design Principles:

- **Focal greenspace, linked visually to the Green Spine**
- **Semi-formal grading to rural edge character**
- **Low-medium density development**
- **Properties to front green space**
- **Equipped play space**
- **SuDS provision (except Home Green)**
- **Part of greenspace network**
- **Seating**
- **Building line / orientation to shape around spaces**

CODE 5.4	Neighbourhood Greens
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	<p>Focal semi-formal greenspaces link between the Green Spine and peripheral Greenway.</p> <p>A predominantly low-medium density arrangement of dwellings orientated to address each of the neighbourhood greens.</p>
<b>Building Frontage</b>	<p>Dwellings to be primarily orientated to face the greenspace.</p> <p>Corner properties to be dual aspect to address the greenspace and street.</p>
<b>Green Space and Planting</b>	<p>Semi-formal planting including some non-native trees, grass and hedgerows close to the residential edges grading to native hedgerow, tree and grassland towards the rural edge.</p> <p>SuDS basins to be either predominantly dry and grass with gentle bank gradients to enable dual use as POS and subject to detailed design planted with native marginal species for any permanently wet areas.</p>

CODE 5.4	Neighbourhood Greens
<b>Urban Form (cont'd)</b>	
<b>Green Space and Planting (cont'd)</b>	Play areas to be LEAPs (and a LAP at Deer Park Green), and designed as a fenced grassed area (LAPs) or equipped play (LEAPs). The play areas may be enclosed by 1.2m high bow top fencing and incorporate suitable robust planting as appropriate. Informal amenity grassed areas will provide for informal play.
<b>Design Parameters</b>	
<b>Building Type</b>	A range of building types from detached to semi-detached and terraced (maximum of three in any one block).
<b>Building Height</b>	Up to 2.5 storeys.
<b>Set Back</b>	Building set-back from edge of highway/shared private drive between 1.5m - 6.2m.
<b>Roof Form</b>	<p>Gable pitched roof, generally running parallel to the street. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.</p> <p>Occasional pitch roof dormers.</p>
<b>Boundary Treatment</b>	<p>Ornamental planting or lawns to dwelling frontages. Corner plot gardens to be edged with single species hedgerow planting.</p> <p>Boundary of green spaces to be defined with native hedgerow planting or timber knee rails as appropriate to reflect the rural edge location.</p> <p>Play areas may be fenced with bow top fencing.</p>
<b>Streets and Parking</b>	<p>Predominantly private drives with parking predominantly to the front of properties and some to the side accessed directly from streets, lanes or shared private drives.</p> <p>Garages, where proposed are to the rear or side.</p> <p>Parking for terraced properties to be front court wherever possible, maximum 4 spaces per group with soft landscaping and street trees between groups.</p>
<b>Materials</b>	The Neighbourhood Greens are distributed throughout the development and as such they fall within either the 'Village', 'Contemporary' or 'Rural' Character Areas as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.

#### 4. Neighbourhood Greens



- 1 Landscaped attenuation
- 2 LAP
- 3 Meadow grassland
- 4 Dwellings front onto open space
- 5 Native woodland planting
- 6 Green space connection to Green Spine





4. Neighbourhood Greens Visualisation

## 5. Greenways

These multifunctional perimeter green corridors provide a transitional landscape between the development edge and the surrounding rural landscape. As well as providing a soft green edge to the development and settlement, they will enhance wildlife corridors through planting and management of native planting and grassland, and will also provide informal recreational routes and connections between greenspaces.



Key Plan (refer to Key Spaces Plan p.71)

### Key Design Principles:

- **Perimeter green recreational corridors up to 15 metres wide**
- **Maintain the boundary network of trees and hedgerows**
- **Maintain and enhance wildlife corridors and incorporate mitigation planting**
- **Native hedgerow and tree planting**
- **Meadow grassland**
- **Strengthen boundaries**
- **Incorporate existing informal recreational routes**
- **Bordered by lanes or shared private drives**
- **New dwellings to overlook the space**
- **Low density development with an informal building line and low degree of enclosure**
- **New informal footpaths**

CODE 5.5	Greenways
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	<p>Linear green corridors around the outer rural edges of the development will incorporate existing trees and hedgerows and be planted with native species to further soften the settlement edge, provide wildlife corridors and mitigation, and provide attractive recreational linkages between larger open spaces, both existing and proposed.</p> <p>Informal, low-density arrangement of dwellings, with variations in building line and orientation and a low degree of enclosure between buildings provides a soft edge to the development.</p>
<b>Building Frontage</b>	<p>Dwellings to be primarily orientated to face the greenspace.</p> <p>Corner properties to be dual aspect to address the greenspace and the lane / shared private drive.</p>
<b>Green Space and Planting</b>	<p>Native tree and hedgerow planting to supplement existing retained vegetation, and meadow grassland.</p> <p>To incorporate a 2m wide informal self-binding gravel (or similar material suitable for multi-use including wheelchairs, prams and cycling) footpath route.</p>

CODE 5.5	Greenways
<b>Design Parameters</b>	
<b>Building Type</b>	Typically detached with some semi-detached.
<b>Building Height</b>	2 storeys.
<b>Set Back</b>	Variation in set-back to accommodate deeper front gardens generally of 4 - 6.2 metres (minimum) from edge of highway / private drive.
<b>Roof Form</b>	Gable pitched roof, generally running parallel to the street. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.
<b>Boundary Treatment</b>	<p>Front gardens to be lawn and ornamental planting or native single species hedgerow.</p> <p>Boundary between outer edge of lane / shared private drive to be mixed native hedgerow or timber knee rail fence.</p>
<b>Streets and Parking</b>	<p>Access directly via a lane or shared private drive.</p> <p>Private drives with parking to the front and / or side of properties.</p> <p>Garages to the rear or side.</p>
<b>Materials</b>	The Greenways pass through the 'Contemporary' and 'Rural' Character Areas as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.

## 5. Greenways



- 1 Landscaped attenuation
- 2 Meadow grassland
- 3 Ecological area - tussock grassland
- 4 Hibernacula
- 5 Dwellings front onto open space
- 6 Informal footpath
- 7 Native hedgerow
- 8 Knee rail





5. Greenways Visualisation

## 6. Pocket Parks

These are small semi-formal greenspaces within the built development area that contain LAPs and / or SuDs features. Dwellings are laid out to overlook and frame the space and provide a local focal point on the street scene.



Key Plan (refer to Key Spaces Plan p.71)

### Key Design Principles:

- Internal smaller, semi-formal greenspaces
- To incorporate a focal building(s) aligned with the space
- Formal arrangement of buildings frame the space
- Buildings orientated to overlook the space
- Medium-higher density development
- Space to be edged with trees
- Spaces incorporate LAPs and / or SuDS
- Seating provided within the space

CODE 5.6	Pocket Parks
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	<p>Internal, smaller, formal spaces enclosed by dwellings, to incorporate LAPs and / or amenity grassed SuDS elements, with wetland planting where appropriate and potentially with semi-ornamental planting where appropriate.</p> <p>Formal medium-high density arrangement of dwellings with a medium-high degree of enclosure between buildings to provide some variation.</p>
<b>Building Frontage</b>	<p>Dwellings to be primarily orientated to face the greenspace.</p> <p>Corner properties to be dual aspect to address the greenspace and the street.</p>
<b>Green Space and Planting</b>	<p>Amenity grass with trees lining the space and occasional focal areas of semi-ornamental planting.</p> <p>Surface treatment of paths to access play facilities to be paved or asphalt.</p> <p>LAPs will be enclosed by 1.2m high bow top fencing and incorporate suitable robust planting as appropriate.</p>

CODE 5.6	Pocket Parks
<b>Design Parameters</b>	
<b>Building Type</b>	Dwellings to be a range of building types from detached to semi-detached and terraced.
<b>Building Height</b>	Up to 2.5 storeys.
<b>Set Back</b>	Building set-back from edge of highway/shared private drive between 1.5m - 4m to create enclosure, and 6.2m (minimum) where parking is proposed to the front of the dwellings.
<b>Roof Form</b>	<p>Gable pitched roof, generally running parallel to the street. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.</p> <p>Occasional pitch roof dormers.</p>
<b>Boundary Treatment</b>	<p>Ornamental planting or lawns to dwelling frontages. Occasional use of estate railings.</p> <p>Boundary of green spaces to be defined by trees, estate railings or hedgerow to road edge. Hedgerow to be maintained at 1m height and planted with temporary, protective post and wire fence to be removed once hedgerow is established.</p> <p>Play areas to be fenced with 1.2m high bow top fencing. The gate to the play area to be distinguished from the play area fence and in a sensitive location to the housing.</p>
<b>Streets and Parking</b>	<p>Garages, where proposed are to the rear or side.</p> <p>Private drives with parking to the front and/or side of properties.</p> <p>Parking for terraced properties to be frontage, 90-degree or parallel, maximum 4 spaces per group with soft landscaping and street trees between groups.</p>
<b>Materials</b>	The Pocket Parks are distributed throughout the development and as such they fall within either the 'Village', 'Contemporary' or 'Rural' Character Areas as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.

## 6. Pocket Parks



- 1 Landscaped attenuation
- 2 LAP
- 3 Dwellings front onto open space
- 4 Hedgerow
- 5 Estate railings





6. Pocket Parks Visualisation

## 7. Focal Squares

Two focal green squares are proposed that define key arrival points and provide navigational aids along the Green Spine route. A formal arrangement of higher density dwellings, with a higher proportion being 2.5 storey, defines the space and creates enclosure. Key changes in materials and use of carefully placed street trees will help to define the character of the space and will assist in providing traffic calming and priority for pedestrians and cyclists.



Key Plan (refer to Key Spaces Plan p.71)

### Key Design Principles:

- Focal green space at two key junctions along the spine road
- To incorporate focal buildings
- Formal arrangement of buildings frame a green space
- Higher density development
- Street trees and formal planting
- Block paved pedestrian priority/traffic tables
- Changes in materials, textures and colours to define the space
- Street trees for traffic calming and shade
- Pedestrian/cycle priority

CODE 5.7	Focal Squares
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	<p>Key focal spaces along the Green Spine, present a sense of arrival and navigation aids.</p> <p>Formal and interesting arrangement of focal buildings predominantly high density around the space, predominantly terraced dwellings with the occasional detached dwelling to provide varying enclosure and interest.</p>
<b>Building Frontage</b>	<p>Dwellings to be primarily orientated to face the square.</p> <p>Corner properties to be dual aspect to address the square and adjoining street.</p>
<b>Green Space and Planting</b>	<p>Green square with formal street trees, mown grass and semi-ornamental shrub planting where appropriate.</p>

CODE 5.7	Focal Squares
<b>Design Parameters</b>	
<b>Building Type</b>	Dwellings to be a range of building types including occasional detached dwellings and terraced.
<b>Building Height</b>	Predominantly 2.5 storeys.
<b>Set Back</b>	Building set-back from edge of highway by 1.5-3m to create enclosure. Limited parking within the Square. Parking to the side or rear.
<b>Roof Form</b>	<p>Gable pitched roof, generally running parallel to the square. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.</p> <p>Flat roof dormers.</p>
<b>Boundary Treatment</b>	Ornamental planting to dwelling frontages and occasional use of estate railings to define garden boundaries adjacent to POS.
<b>Streets and Parking</b>	<p>Space defined through use of materials and / or block paved table top traffic calming detailing.</p> <p>Garages, where proposed are to the rear or side.</p> <p>Limited parking within the square for terrace properties, 90-degree or parallel, front court. Majority of parking to the rear of the Square overlooked by properties. Maximum 4 spaces per group with soft landscaping and street trees between groups.</p>
<b>Materials</b>	Two Focal Squares are proposed. They fall within the 'Contemporary' Character Area as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.

## 7. Focal Squares



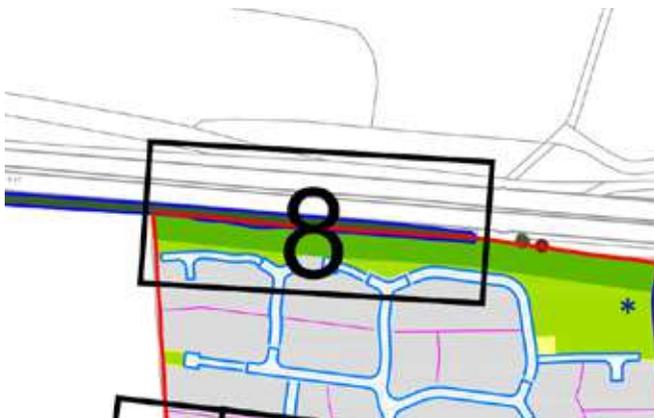
- 1 Landscaped square
- 2 Dwellings front onto open space
- 3 Focal properties in street views
- 4 Green space connection to Green Spine
- 5 Landscaped swale
- 6 Combined pedestrian and cycle path
- 7 Estate railings
- 8 LAP



7. Focal Squares Visualisation

## 8. A47 Corridor

A noise and landscape buffer are required between the A47 and the northern edge of the development. Noise engineer advice is to provide a minimum 15 metre zone with a continuous and unbroken earth bund or a bund/fence combination to an overall height of 4m from site grade along the entire boundary with the A47 with the first line of dwellings to face the northern boundary and present a continuous barrier to shield rear gardens and streets behind from noise. The noise barrier proposals will need to be consistent and tie-in with the proposals for the adjacent development site.



Key Plan (refer to Key Spaces Plan p.71)

### Key Design Principles:

- **15 metre landscape buffer zone along northern boundary**
- **4m high noise barrier**
- **Native tree and shrub planting to bund**
- **High density arrangement along northern built edge**
- **2 storey dwellings**
- **Properties to face the A47**
- **Building line to be at least 20m from the site boundary**
- **Terraced or semi-detached properties and linked garages to provide a continuous barrier**
- **High performance glazing and ventilation to '1st row' of dwellings closest to northern boundary**

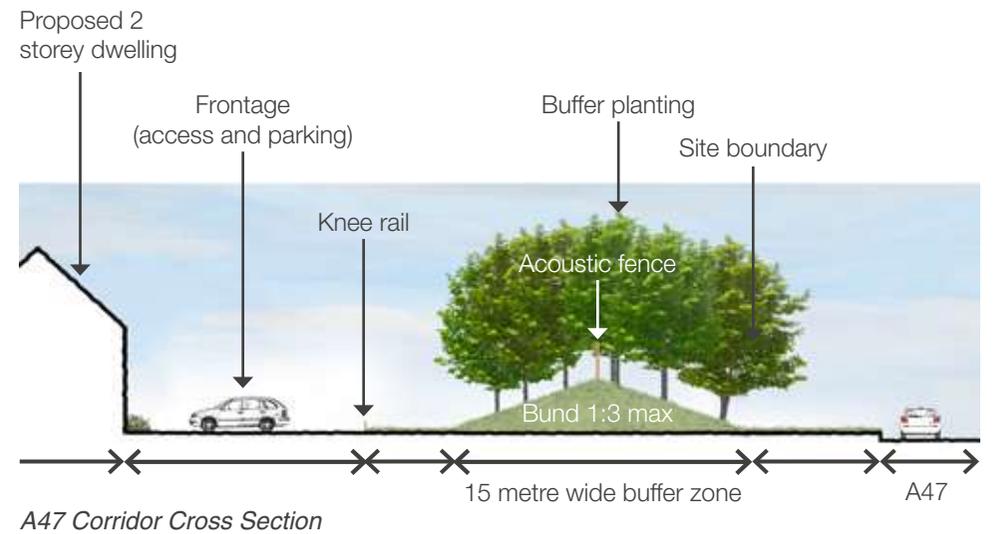
CODE 5.7	A47 Corridor
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	<p>A noise and landscape buffer are required between the A47 and the northern edge of the development.</p> <p>Higher density arrangement of buildings to face towards the A47 and provide a continuous barrier to minimise noise on gardens, streets and POS behind.</p>
<b>Building Frontage</b>	<p>Dwellings to be orientated to face the A47, with frontages no closer than 20m from the northern site boundary.</p> <p>A generally continuous frontage of dwellings with linked garages.</p>
<b>Green Space and Planting</b>	<p>The landscape buffer to be a minimum of 15m depth and will incorporate a 4metre high bund and noise fence combination, with native tree and shrub planting to both sides of the bund.</p>

CODE 5.8	A47 Corridor
<b>Design Parameters</b>	
<b>Building Type</b>	Terraced and semi-detached with linked garages (where proposed).
<b>Building Height</b>	2 storeys.
<b>Set Back</b>	Dwellings to be set back a minimum of 20 metres from the site boundary (5 metres from the 15m buffer zone) to meet noise requirements. Building set-back from edge of highway/shared private drive between 1.5m - 6.2m (minimum) to allow for frontage parking where proposed.
<b>Roof Form</b>	Gable pitched roof, generally running parallel to the street. Occasional gable end facing the street can be used with variation in roof shape and/or pitch to create feature or for turning corner.
<b>Boundary Treatment</b>	Ornamental planting to dwelling frontages. Occasional trees in front gardens where space permits. Any visible side boundaries or gaps between properties to be 1800mm high brick garden walls or closeboard fencing.
<b>Streets and Parking</b>	Accessed via a lane or shared private drive. Primarily private drives with parking to the front and/or side of properties. Some court parking may be required for terraced properties. This will be to the front wherever possible and may be 90-degree or parallel, maximum 6 spaces per group with soft landscaping and street trees between groups.
<b>Materials</b>	The A47 Corridor is located within a 'Village' Character Area as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.

## 8. A47 Corridor



- 1 A47
- 2 Acoustic bund with fence and native planting
- 3 Wall of dwellings to provide additional acoustic barrier to gardens and development to the south
- 4 Knee rail to edge of private drive to prevent parking
- 5 Timber bollard to edge of lane to prevent parking





8. A47 Corridor Visualisation

## 9. Settlement Interface

The approved Illustrative Masterplan proposes a planted landscape buffer to the rear of properties along the following streets where they back on to the site; Woodview Road, Buxton Close, Parker's Close and Dereham Road.

Existing hedgerows and/or trees along the boundaries of Buxton Close (south) and Cardinal Close with the site will be retained and reinforced with further planting if required.

The form the new buffer will take has been carefully considered in order to address the potential for views and inter-visibility between existing and new properties, security for existing and proposed dwellings, potential for overshadowing by proposed buffer planting, and to allow for successful establishment, maintenance and review of its function with residents. The following proposals represents the most practical design solution and has been requested by South Norfolk District Council.

### Key Design Principles:

- **New dwellings to be set back from site boundary at right angle, and accessed via shared private drives**
- **Majority of existing properties will therefore back onto rear gardens rather than dwellings**
- **In some locations properties closest to the boundary to be bungalows**
- **10-metre-wide single ownership landscape buffer alongside boundary between gardens**
- **Buffer to incorporate a native hedgerow with trees (which provides benefits for wildlife such as hedgehogs and bats) planted alongside the site boundary and be sufficiently wide to allow for maintenance access**
- **Secure gated maintenance access to be provided at intervals**
- **No public access**
- **Rear garden boundaries to be 1.8m open board fence so that the buffer strip is visible from gardens – for security purposes and to discourage tipping of garden waste**
- **Management plan to require maintenance contractor to report any encroachment / fly tipping from private gardens**
- **Hedgerow to be maintained at 3-4 metre height to prevent overshadowing of existing properties/ gardens**
- **Opportunity for review of managed hedgerow height with residents to be included within the management plan**



Key Plan (refer to Key Spaces Plan p.71)

CODE 5.9	Settlement Interface
<b>Urban Form</b>	
<b>Spatial Arrangement</b>	<p>A landscape buffer is required to the rear of existing properties identified on the adjacent plan, where they back on to the site.</p> <p>New dwellings to be primarily arranged at right angles to the site boundary, accessed via shared private drives to minimise numbers of dwellings that will back on to the site boundary.</p>
<b>Building Frontage</b>	<p>Dwellings to be side on to the rear gardens of existing properties and will provide a closed-block arrangement in accordance with Secured by Design principles.</p>
<b>Green Space and Planting</b>	<p>A 10-metre wide single ownership landscape buffer is proposed between rear gardens of existing and gardens of proposed properties.</p> <p>Buffer to incorporate a native hedgerow planted alongside the site boundary. Tree planting to be undertaken to the end of existing property gardens (one tree per garden) unless not required by property owner.</p>
<b>Design Parameters</b>	
<b>Building Type</b>	<p>Detached, semi-detached, and occasional terracing.</p>
<b>Building Height</b>	<p>2 storeys. In some locations properties closest to the boundary to be bungalows.</p>
<b>Set Back</b>	<p>Dwellings to be side on to the site boundary so that the majority of existing properties will back on to gardens rather than dwellings.</p> <p>Building set-back from edge of highway/shared private drive between 1.5m - 6.2m (minimum) to allow for frontage parking mews where proposed.</p>
<b>Roof Form</b>	<p>Gable pitched roof, generally running parallel to the shared private drive.</p>
<b>Boundary Treatment</b>	<p>Rear garden boundaries to be 1.8m open board fence.</p> <p>Ornamental planting to dwelling frontages.</p> <p>Occasional trees in front gardens where space permits.</p> <p>Any visible side boundaries or gaps between properties to be 1800mm high brick garden walls or closeboard fencing.</p>
<b>Streets and Parking</b>	<p>Shared private drives with on-plot parking and garages accessed directly from a secondary street or lane.</p>
<b>Materials</b>	<p>The Settlement Interface is located primarily within a 'Village' Character Area, with limited areas defined as 'Contemporary' as indicated on the plan at page 55. Refer to Section 6 Design Codes for the materials palettes for this character area.</p>

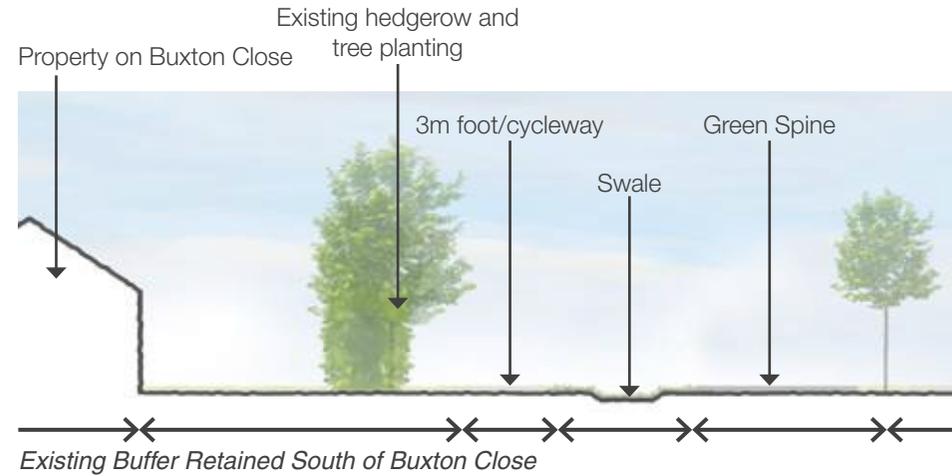
## 9. Settlement Interface



- 1 Landscaped edge, 10m wide with native hedgerow and tree planting where required adjacent to rear property boundaries
- 2 1.8m hit and miss fence prevents public access for security
- 3 Bungalows proposed adjacent to site boundary



9. Settlement Interface Visualisation



## Section 6

# DETAILING THE PLACE

### Design & Materials – Built Form

The architectural response to the development aims to create houses for the 21st Century. Simple, elegant and traditional styles are to be used in order to create a place that is both distinctive and individual.

The architectural parameters established in this section relate to building detailing such as roofs, windows and doors and the type and mix of building materials to be used.

Design Code 6.1 defines the materials palette. A mix of brickwork, rendered weather board cladding is proposed with a limited palette of colour and textures that will compliment the character of Easton and will vary to define the different character areas defined across the development. A 'V', 'R', or 'C' is indicated for each item within the palette at Code 6.1 to define which character area it is applicable to.

#### **Key Design Principles:**

- **Simple form utilising well-proportioned buildings and a refined materials palette.**
- **A range of house types will be utilised across the development, creating attractive street scenes with variations included at key locations and adjacent to amenity open spaces.**
- **High quality materials are to reflect the range of treatments throughout Easton and as defined and recommended within the South Norfolk Placemaking Guide.**
- **Details should not distinguish between market and affordable housing.**
- **The development will provide for areas of contemporary, rural edge and village style housing. Broad locations for each of these areas are indicated in the adjacent plan.**



*Key Plan*

CODE 6.1

Materials Palette

Brick



- 1. Red
- 2. Red Multistock
- 3. Mottled Red
- 4. Buff Multistock
- 5. Gault Buff

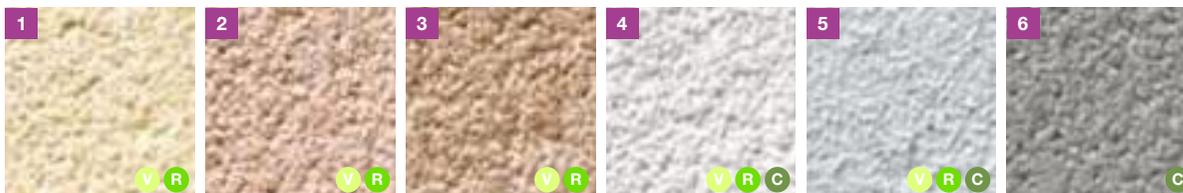
Weather Board/Cladding (RCM Supertech)



- 1. Jet Black
- 2. Light Ivory
- 3. Grey Brown
- 4. Light Grey
- 5. Blue Grey
- 6. Steel Blue
- 7. Dark Grey\*

\*No's 1, 5 and 7 be used with red or buff brick, No's 2, 3 and 4 to be used with red brick with No 6 to be used with buff brick only.

Render (K-Rend)

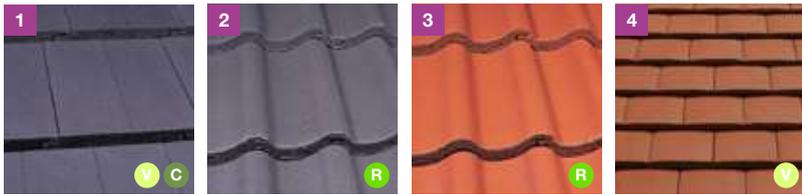


- 1. Arran
- 2. Cinnamon
- 3. Fintry Stone
- 4. White
- 5. Powder Blue
- 6. Pewter Grey

CODE 6.1

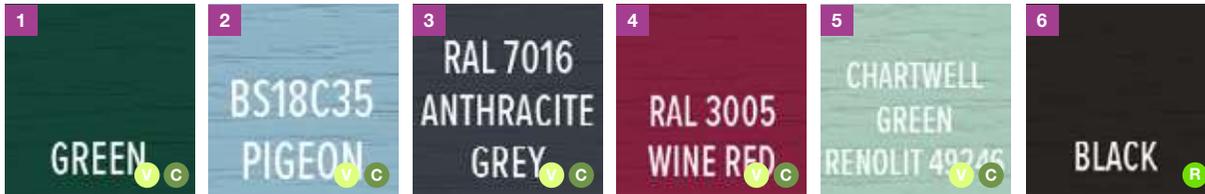
Materials Palette

Roof Covering



- 1. Concrete Tiles - grey flat - profile
- 2. Grey Concrete Pantiles
- 3. Red Concrete Pantiles
- 4. Red Flat Profile Tiles

Front Doors (IG Doors - GRP)



- 1. Green
- 2. Pigeon
- 3. Anthracite Grey
- 4. Wine Red
- 5. Chartwell Green
- 6. Black

Windows



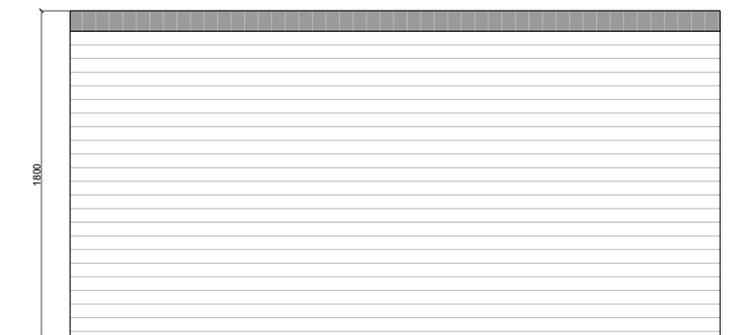
- 1. Plain Casement - Anthracite/D.Grey
- 2. Horizontal Glazing Bar - White

## Boundary Treatments

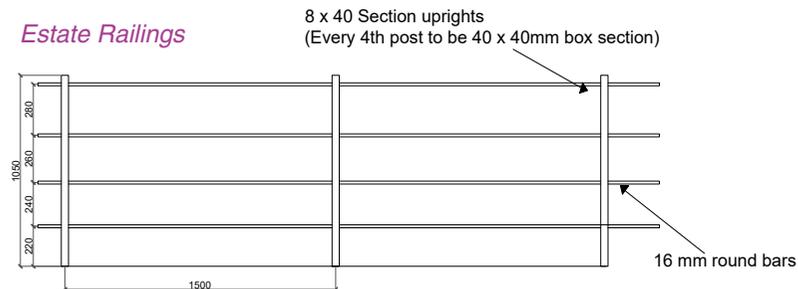
The design and treatment of boundaries helps to define the overall character of a place. The following Design Code sets out the range of boundaries proposed and their design parameters. The range of boundaries proposed reflects the prevailing character within Easton and the surrounding rural context to the site.

CODE 6.2	Boundary Treatments
<b>Front Garden Boundaries</b>	
<b>No Boundary (Open Frontage)</b>	<p>Change of surface treatment or grass / ornamental planting utilised to define defensible private space to property frontage.</p> <p>Treatment dependent on depth of frontage.</p> <p>Narrow frontage e.g. 1-2m depth to be an ornamental planting bed.</p> <p>Wider frontage may be lawn and / or low (&lt;1metre) ornamental planting.</p>
<b>Hedgerow Boundary</b>	<p>Single species hedgerows e.g. lavender (formal/low level), hornbeam (rural edge), photinia or Prunus laurocerasus 'Otto Luyken' (colour/flowering). Hedgerow type to be utilised to assist in defining character areas.</p> <p>Do not plant within vehicular or pedestrian visibility splays.</p> <p>Allow for 1.5m x 1.5m visibility splay between driveway access and highway boundary.</p>
<b>800mm Brick Wall</b>	<p>Ensure walls are not located within visibility splays.</p> <p>Matching brick on edge coping detail.</p>
<b>Estate Railings</b>	<p>1050mm height.</p> <p>Galvanised and painted black.</p> <p>Gates if provided must match railings in style, colour and material.</p> <p>Limited use at key entrance locations, within rural character areas.</p>

*Brick Garden Wall*

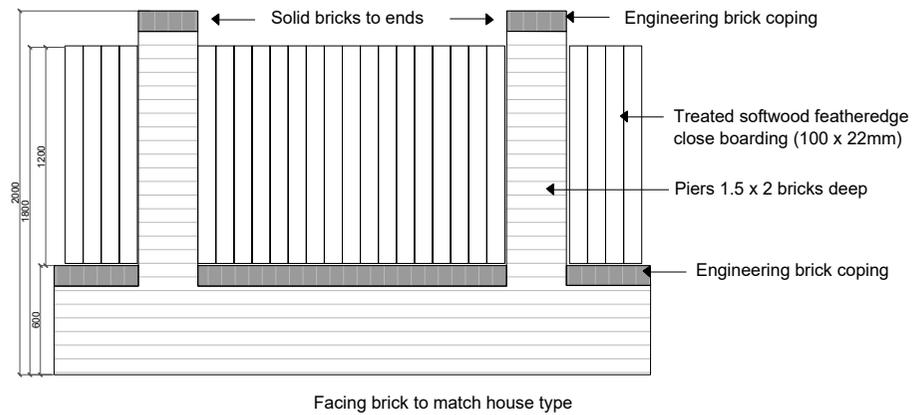


*Estate Railings*

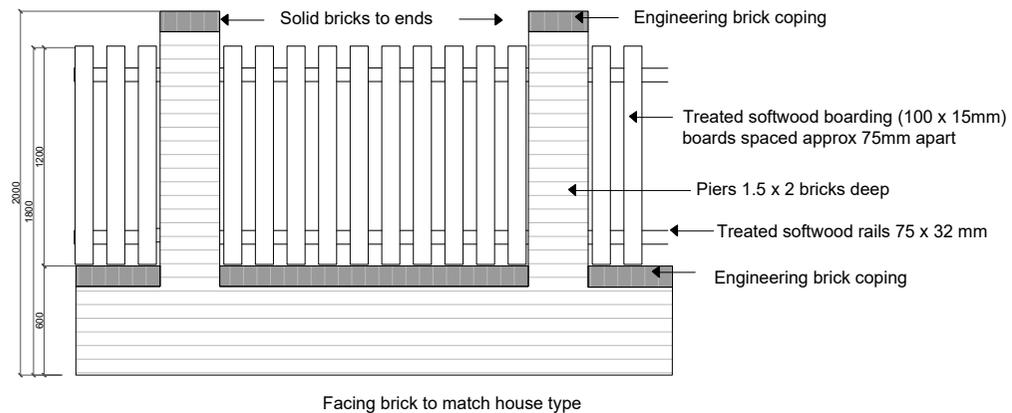


CODE 6.2	Boundary Treatments
<b>Rear / Side Boundaries</b>	
<b>Brick Wall</b>	<p>1.8m high</p> <p>Matching brick on edge coping detail.</p> <p>Long stretches of wall will not be provided opposite each other on the street scene</p>
<b>Brick Wall with Pillars and Fence</b>	<p>Wall to be 600mm high.</p> <p>Matching brick on edge coping detail.</p> <p>Fence to be either open boarded or close boarded and 1.2m high.</p> <p>Total wall height 2m to top of pillars.</p> <p>Use at side boundaries where they are more prominent on the street scene.</p>
<b>Timber Fencing</b>	<p>1.8m high open-boarded fence panel or close-boarded fence (no gravel board).</p> <p>Ensure sufficient space at the base for hedgehog access. A 13x13cm gap will allow a sufficient space for a hedgehog but be too small for most pets.</p> <p>To be used in locations where less visible.</p>

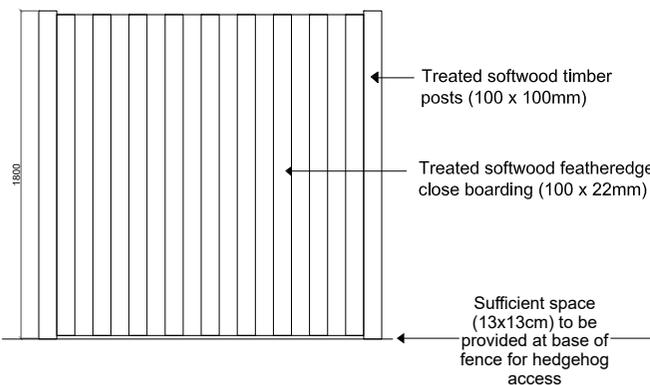
### Brick Walls with Pillars and Fence



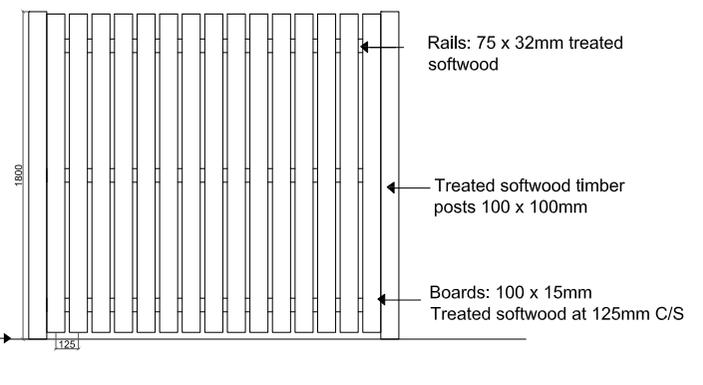
### Open-boarded fence on wall



### Close-boarded Fence

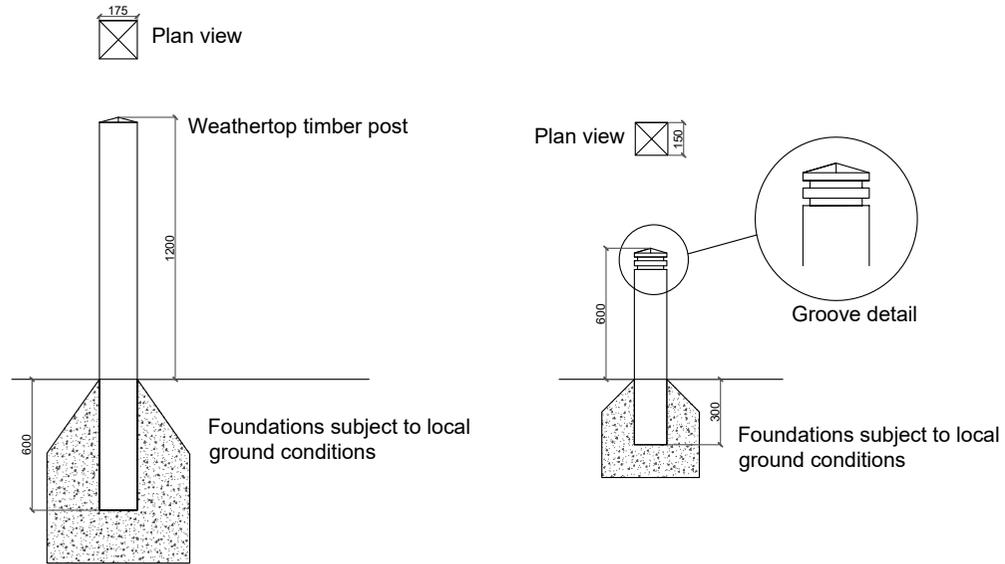


### Open-boarded Fence

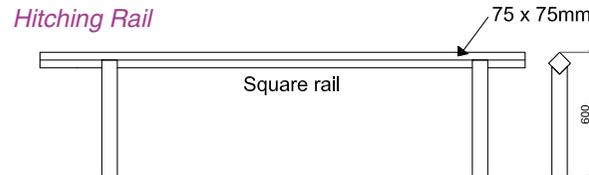
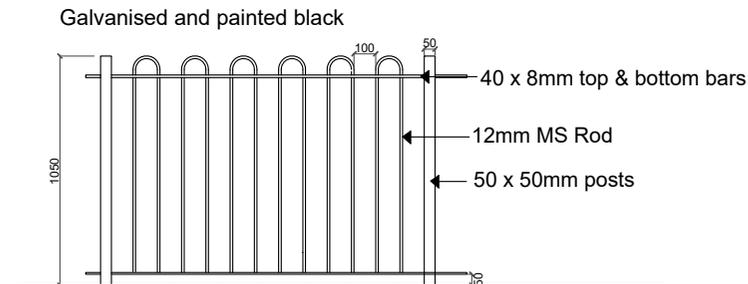


CODE 6.2	Open Space Boundaries
<b>Open Space Boundaries</b>	
<b>Knee Rail or Bollards</b>	<p>Timber knee rail 600mm high or timber bollards (bollards are preferred to encourage permeability for people).</p> <p>To edges of open space areas where required to prevent parking.</p>
<b>Hedgerows</b>	<p>Native mixed species hedgerows.</p> <p>To be cut back to a height of 1.2 metres and maintained at below average head height where visibility is required.</p> <p>To outer edges of shared private drives where they adjoin the peripheral greenways.</p> <p>Hedgerow planting to be protected by a simple timber post and 3 wire fence which will be removed once the hedgerow is established.</p>
<b>Estate Railings</b>	<p>1050mm height.</p> <p>Galvanised and painted black.</p> <p>Gates if provided must match railings in style, colour and material.</p> <p>Limited use at key entrance locations, within rural character areas.</p>
<b>Bow-top Railings</b>	<p>1050mm high.</p> <p>Galvanised and painted black.</p> <p>To be used around play areas where they are to be enclosed.</p> <p>A suitable matching gate of contrasting colour will be required.</p>

### Feature Timber Posts



### Bow-top Railings



## Parking

New homes and community facilities will be designed so that they have sufficient cycle and car parking provision based upon the Parking Standards for Norfolk Guide (2007), Manual for Streets 1 & 2 and will be in accordance with the relevant policy including the Easton Neighbourhood Plan.

Careful detailing in terms of the plot arrangement, frontages, landscape boundary treatments, street alignment and surface treatments will help to sensitively integrate vehicles into the layout to create attractive street scenes that are not dominated by parked vehicles.

### **Car Parking**

The South Norfolk Placemaking Guide (section 3.7.2) requires that the design of parking provision must balance a number of issues, including being:

- **close to dwellings;**
- **accessed from front doors where possible;**
- **overlooked by dwellings; and**
- **supporting the character of the place.**



*Front and side parking example*

A range of residential parking methods are proposed in order to accommodate car use without dominating the environment, these primarily comprise on-plot driveways some with garages and frontage parking. Frontage parking to be limited to 4 spaces, or 8 if interspersed with landscaping. Frontage parking to be provided to one side of the street only. Communal rear courtyard parking should be avoided wherever practicable, where unavoidable it must be demonstrated that they will be secure and overlooked from habitable rooms and well-related / integrated with the dwellings they serve.

For the communal facilities on-plot car parking will be considered by the providers.

### **Cycle Parking**

For community uses and key public open spaces (the Greens), secure cycle parking is required and provision will be agreed with the LPA.

CODE 6.3	Parking
<b>Visitor Parallel Parking</b>	<p>2x6 metre bays.</p> <p>Maximum of 4 spaces per group.</p> <p>Soft landscaping and street trees between groups.</p> <p>Parking bays will be of the same material as the adjacent carriageway. Allocated parking bays may need to be defined by a contrasting edge colour / material.</p>
<b>Frontage (90 degree) Parking</b>	<p>2.5x5 metre bays.</p> <p>6m clearance for turning (appropriate to the green spine/secondary streets/squares only).</p> <p>Maximum of 4 spaces per group.</p> <p>Soft landscaping and street trees between groups.</p> <p>Parking bays will be of the same material as the adjacent carriageway except within the Feature Squares. Allocated parking bays may need to be defined by a contrasting edge colour / material.</p>
<b>On-plot Parking</b>	<p>2.5x5 metre bays (plus appropriate clearance).</p> <p>Garages set back minimum 6m from back edge of highway / footpath (to rear or side of dwelling unit).</p> <p>Garages internal dimension 6x3m.</p> <p>Garages may be integral.</p> <p>Driveways will typically be of contrasting material to the adjacent carriageway - generally they will be block paved.</p>
<b>Courtyards</b>	<p>Careful design to ensure overlooking from habitable rooms and ease of access for residents.</p> <p>Public realm space with soft landscaping.</p>
<b>Parking for community areas</b>	<p>Small public realm car parks for the village hall and shop to be softened by planting - delivered by others.</p> <p>Limited on street parallel parking bays adjacent to public open space (see above for parallel parking principles).</p> <p>Laid out to allow safe pedestrian access to and from vehicles.</p> <p>To include disabled badge holder and parent and child spaces.</p> <p>To include cycle and motorcycle spaces.</p>

## Public Realm and Street Furniture

Street furniture and hard landscape materials and surfaces should be well chosen, being practical and robust in design. A selected palette of materials should be adopted so that streets, routes, surfaces and crossing points are legible. This should be based on inclusive design with a particular focus on less mobile people, people with dementia, and the visually impaired. At reserved matters stage consideration will be given to the provision of/and location of defibrillators in publicly accessible locations.

### **Key Design Principles:**

- **Simple contemporary form in keeping with the 21st century aspiration of the wider development**
- **Timber elements of furniture to have natural finish and be of a chunky, robust aesthetic**
- **Metal elements of furniture to be unfussy with a black gloss finish**
- **Railings to be estate railings or vertical bars (black). Further Detail on boundary treatments is provided within Code 6.2**
- **Lighting columns (if required), bollards and signage to be of a simple, contemporary design either in wood or in metal with black gloss finish**
- **Footpaths through public open spaces with the potential to be heavily trafficked will be black asphalt**
- **Footpaths through informal peripheral greenways will use a suitable self-binding natural stone material such as hoggin or compacted limestone, less durable than asphalt, the specification must be sufficient to allow for incidental cycle, push & wheelchair use**
- **Paved areas within key spaces may include some block paving detail**

**CODE 6.4**

**Street Furniture**



- 1. Litter Bin E.g. Woodscape 'LBR 90 Round Lockable'\*
- 2. Seat E.g. Woodscape 'Tooting Backrest Seat'\*
- 3. Fingerpost Directional Sign
- 4. Information Board
- 5. Seat E.g. Broxap 'Litchard Seat'
- 6. Estate Railing
- 7. Timber Bollard
- 8. Timber Knee Rail
- 9. Dog Bin E.g. JRB 'Hercules' \*or similar approved

A limited palette of robust street furniture is proposed. Street furniture will be strategically located only where it is needed, with the aim of minimising clutter on the street-scene. The public open space landscape plans will provide this detail. A few bench types will be permitted that reflect the location of the bench e.g. within the 'Contemporary', 'Rural' or 'Village' Character Areas, benches to include back and arm rests to ensure suitability for people who are less mobile.

**CODE 6.5**

**Hard Landscape**

*Pedestrian/Cycle Routes - Pathways*



*Social Spaces*



- 1. Black Asphalt
- 2. Crushed Stone
- 3. Compacted Limestone
- 4. Whin Dust Path
- 5. Brindle Pavers
- 6. Charcoal Pavers

A range of durable surfaces that help to facilitate access for all will be provided across the publicly accessible routes and spaces. Refer to the Design Codes in Sections 4 and 5 and to the landscape proposals plans for further detail.

## Waste and Recycling

Careful consideration will be given to the location and nature of storage areas for refuse and materials for recycling so as not to detract from the appearance of the development.

CODE 6.6	Waste and Recycling
Houses	<p>Each house will have sufficient space within the curtilage for three 240-litre wheeled bins (blue- recyclable material, brown-garden waste, green-residual waste).</p> <p>To be easily accessible from the property.</p> <p>To be in a location that allows the bin to be easily moved (via accessways of sufficient width) to the street for collection.</p> <p>To be located behind the building line and preferably out of view from the street.</p>

## Other Key Design Considerations

### Inclusive Access

The masterplan has been developed to provide a network of accessible routes through the site. This is facilitated by the generally flat nature of the site. All footpaths and 'green' routes will comply with the statutory requirements using the gradients contained in Part M of the Building Regulations as the maximum where possible.

### Dementia Friendly Community Design

The 'At a Glance' Checklist prepared by the Housing Learning Improvement Network (June 2012) recommends 17 key design features of new dementia friendly neighbourhoods. Where applicable, these design features will be incorporated within the layout. Many are similar to the requirements of 'Building for Life', which is referred to on the following page.

### Secured by Design

Creating a safe place to live and play is fundamental to the scheme design. This will be achieved by the way the development is laid out and by the detailed street, block and plot design. Active frontages will face streets and public areas in order to promote 24-hour surveillance, and to encourage safer places. Public areas such as Streets, Green Space and Play Areas will be designed so that they are safe, easily accessible and attractive to use. All users will be considered as part an inclusive design approach.

Surveillance of public spaces by a number of properties and buildings is important. Barriers, blank walls and 'dead ends' will be avoided. Windows and doors should be included within corners or gable ends where they overlook public open space. Direct, well-lit streets and routes are essential and this will be adopted. Across the whole development careful detailed attention will be paid to designing out crime through the layout, and promoting privacy and security.

This will be achieved by; a secure closed-block masterplan design; high quality active streets; the position of buildings to the front of the plot; well-located windows and doors that survey the public realm (including windows in side elevations on dual aspect corner properties); clearly defining public and private space; well-chosen use and location of boundary details, and careful use of planting in the public realm. Long lengths of inactive frontages on the street should be avoided i.e. avoid long sections of walls/high fences on opposing sides of the street.

“Secured by Design”, “Planning & Access for Disabled People” and “Manual for Streets” provide sources of design reference in relation to safe places and designing out crime. They will be thoroughly embraced during the detailed design stage.

#### **Key Design Principles:**

- **High quality, fully active streets;**
- **The position of buildings to overlook the streets and public spaces;**
- **Well located windows and doors that survey the public realm;**
- **Clearly defining public and private space;**
- **The well-chosen use and location of boundary details to establish secure plots;**
- **Careful use of planting in the public realm.**

#### **Building for Life**

Policy 2 (Promoting Good Design) of the Joint Core Strategy for Broadland, Norwich and South Norfolk (JCS) 2011/2014 requires development to be evaluated against the Building for Life criteria published by CABI, achieving at least 14 points (silver standard).

A Building for Life compliance statement will be included with each Reserved Matters application.

The outline application for the proposed development was subject to a Building for Life Assessment by the South Norfolk Council Design Officer. The design principles, established for the outline application carry through within the Design Code. The Design Code further develops principles in relation to the built character of the development and design to encourage low vehicle speeds.

## Section 7

# DELIVERY

### Phasing

This section sets out the phasing, implementation and management of the development.

Persimmon Homes propose to develop the site within five phases as shown on the adjacent plan. The sequencing of construction will commence within the east of the site, south of Dereham Road and rotate anti-clockwise through the site, with the final phase being north of Dereham Road.

The primary road infrastructure provides linkages between existing roads that head south from Easton and therefore can readily be constructed in phases in-line with the residential parcels to ensure good accessibility for vehicles, pedestrians and cycles at all stages of development. The soft landscaping and tree planting works within the green spine will be undertaken following the completion of the base course.

Each phase is set out to incorporate adequate public open space and play facilities to meet the needs of residents

and to include SuDS provision to ensure surface water can continue to be released from the site at greenfield run off rates. The general principle for the delivery of the public open space will be for hard landscaping elements to be laid out in conjunction with the construction of dwellings within the phase and soft landscaping to be completed within the following planting season.

For the central 'Easton Green' and adjacent village hall, school extension and shop, timescales for delivery to be determined dependent on provision of supporting infrastructure and likely demand for these services.

The new allotments with access will be constructed and populated prior to the commencement of the Phase 4 development to allow allotment holders time to transition between old and new smallholdings.

### Affordable Housing

Affordable housing will be evenly distributed throughout each phase of the development and will be provided as set out within the S106 Agreement. The distribution of affordable housing will be determined at the reserved matters stage for each phase of the development following input from the enabling officer.

The market housing will be privately owned. Affordable housing will be owned and managed by the appointed registered social landlords.







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